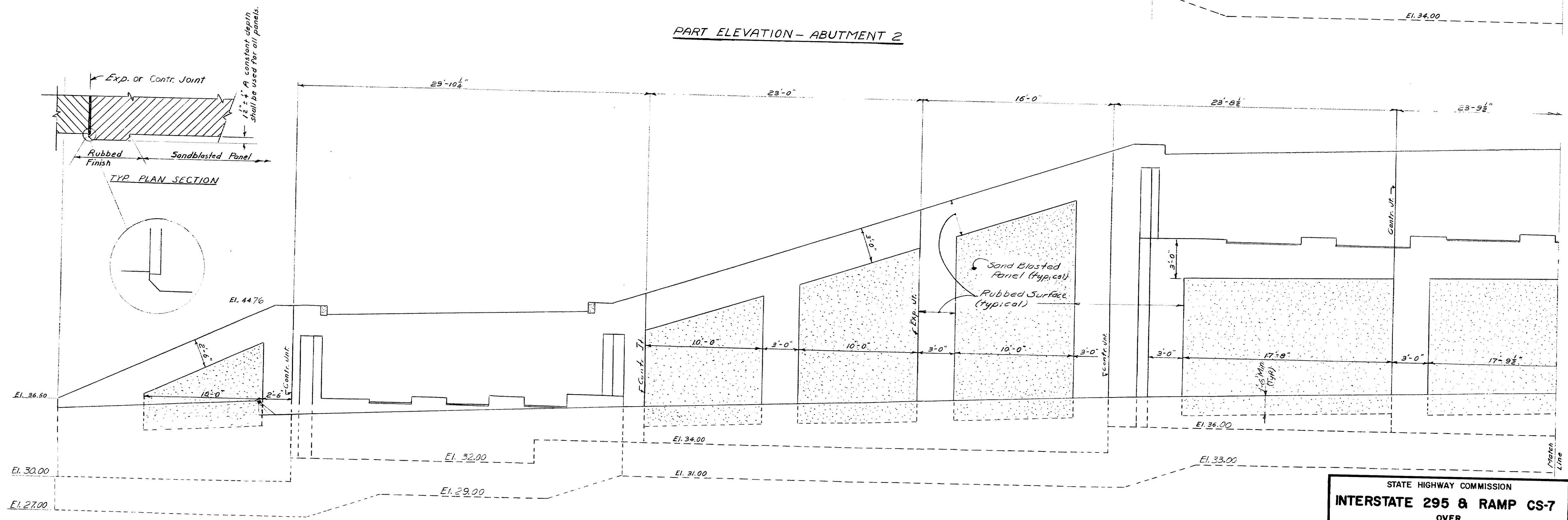
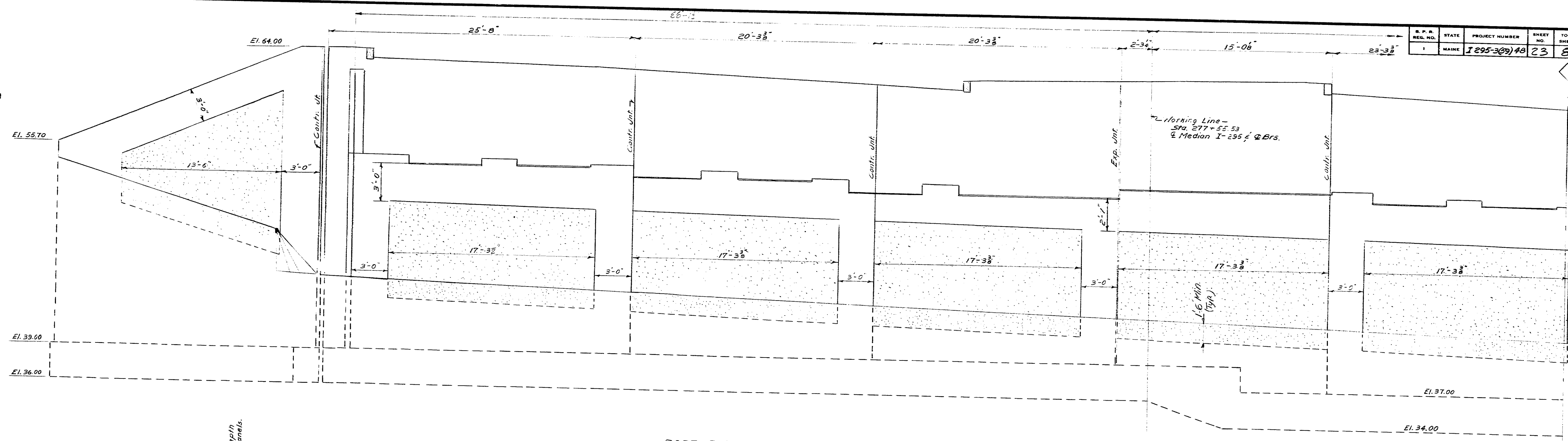


S.P. R.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	I 295-3(9) 48	23	84

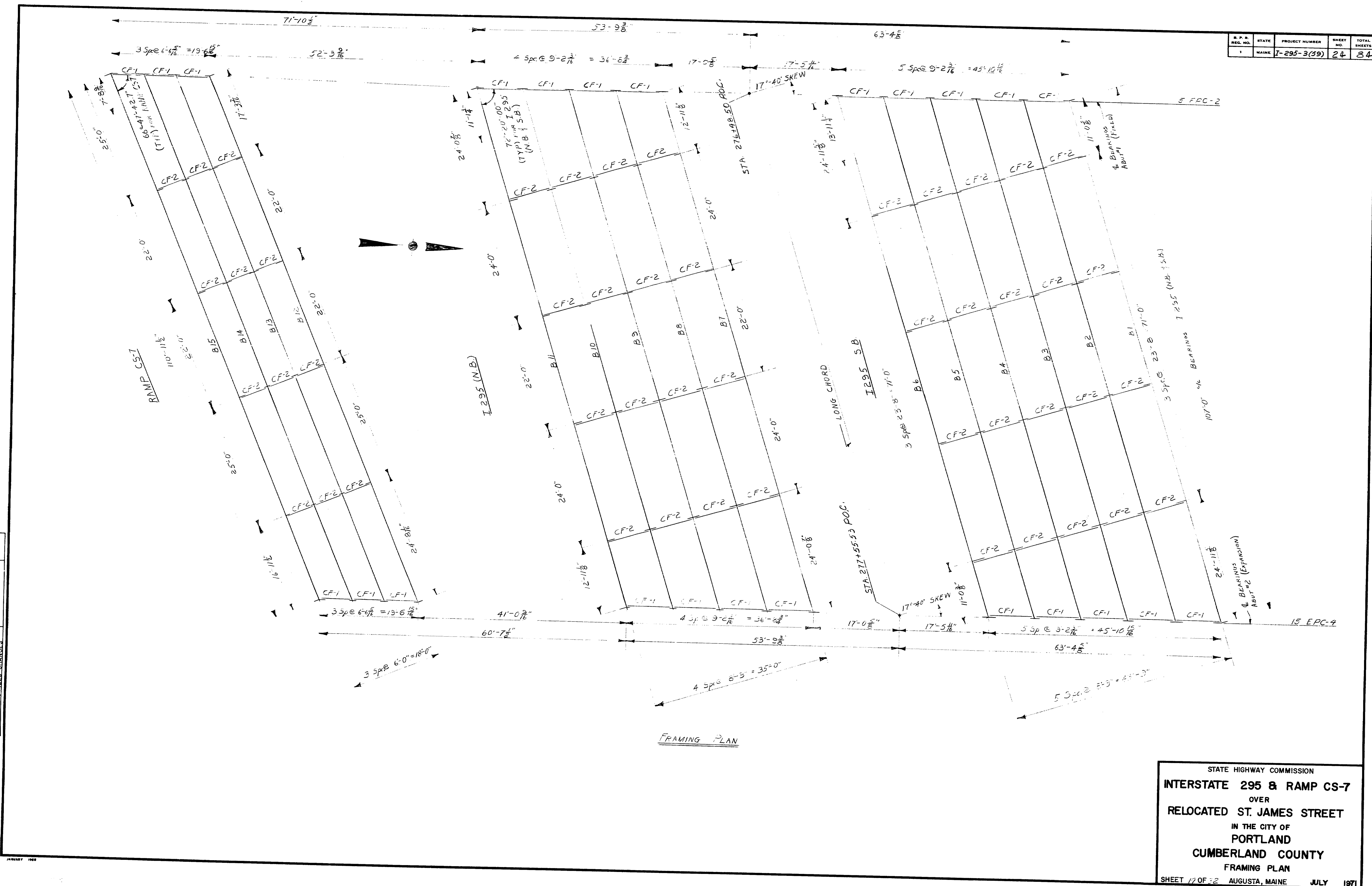


PLANS	DATE	BY	CHECKED	DATE
DESIGN	11/17	CH		
CHECKED	12/12	W.E.A.		
REVISIONS				
FIELD CHANGES				

STATE HIGHWAY COMMISSION
INTERSTATE 295 & RAMP CS-7
 OVER
RELOCATED ST. JAMES STREET
 IN THE CITY OF
PORTLAND
CUMBERLAND COUNTY
 ARCHITECTURAL TREATMENT
 SHEET 16 OF 32 AUGUSTA, MAINE JULY 1971

153-55

PLANS	BY		DATE
	DESIGN - DETAIL	MMG	7-77
	CHECKED	M.F.A.	7-30-77
	REVISIONS		
	FIELD CHANGES		

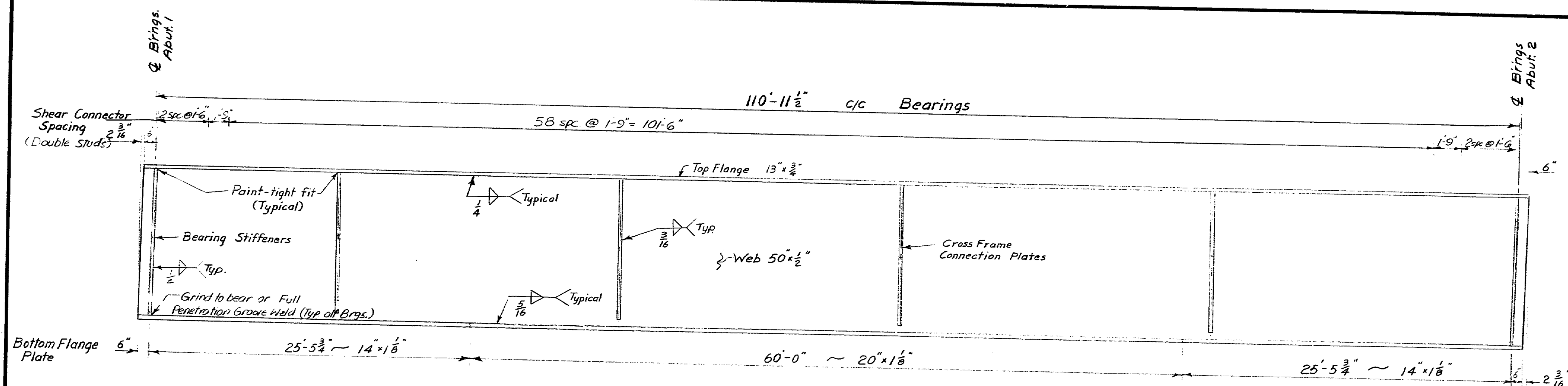


S.P.R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	I-295-3(39)	24	84

STATE HIGHWAY COMMISSION
INTERSTATE 295 & RAMP CS-7
 OVER
RELOCATED ST. JAMES STREET
 IN THE CITY OF
PORTLAND
CUMBERLAND COUNTY
 FRAMING PLAN
 SHEET 17 OF 22 AUGUSTA, MAINE JULY 1971

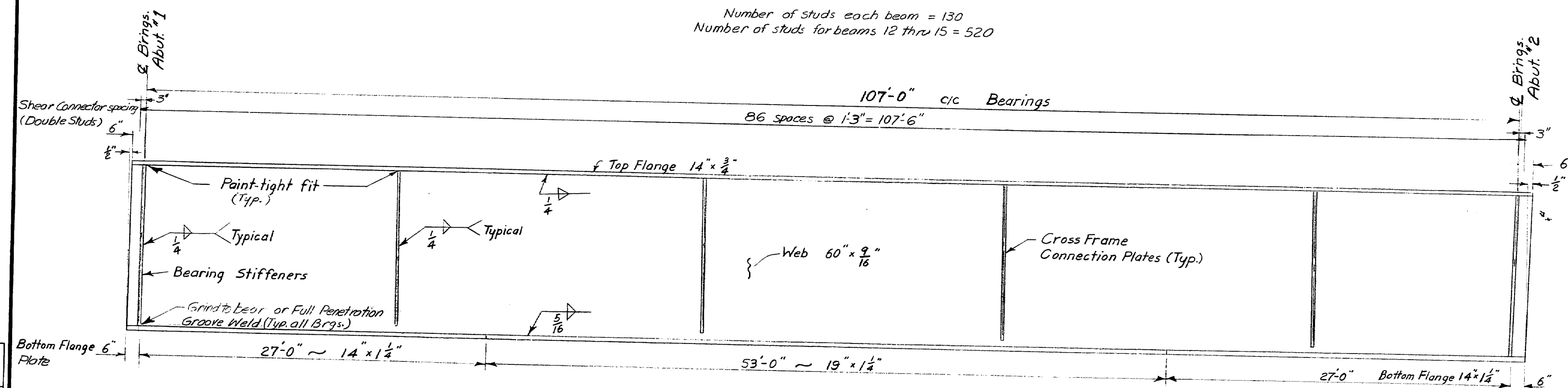
153-56

S.P.R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	I-295-3 (59)	25	84



ELEVATION ~ BEAMS 12 - 15

Number of studs each beam = 130
Number of studs for beams 12 thru 15 = 520



ELEVATION ~ BEAMS 1 - 11

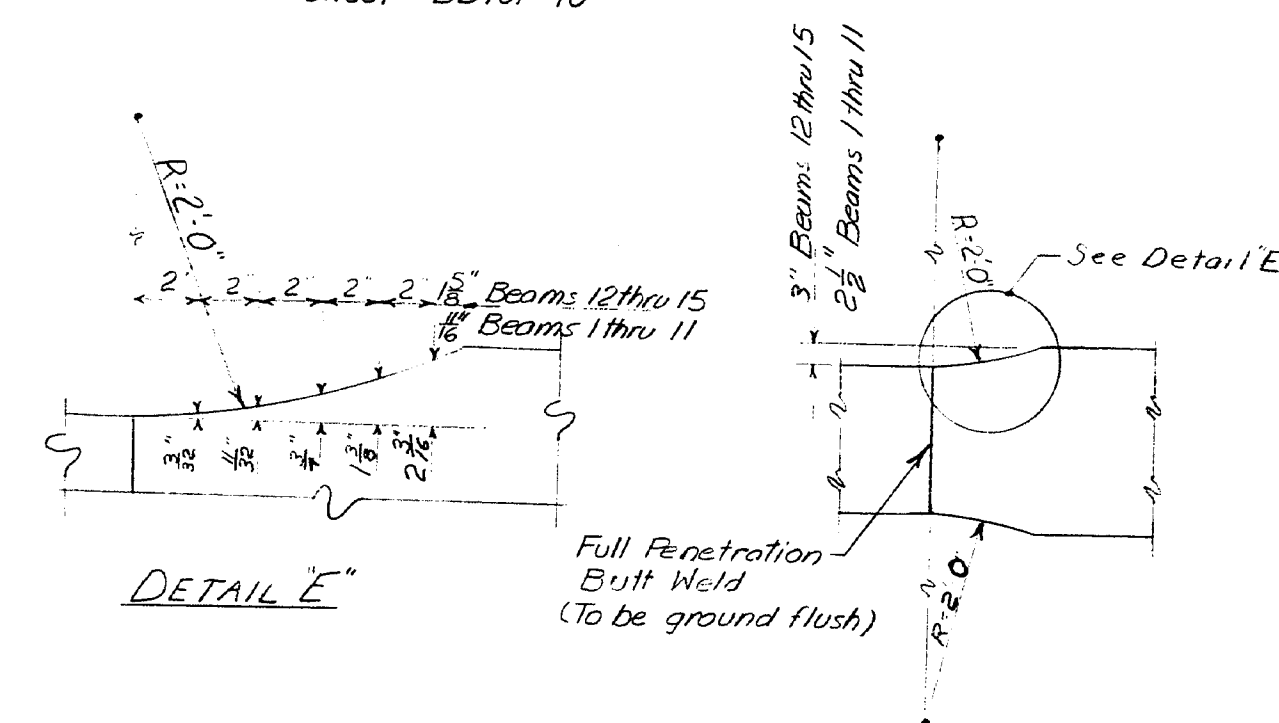
Number of studs each beam = 174
Number of studs for beams 1 thru 11 = 1914
Total number of studs for all beams = 2434
Wt. of studs = 2378 lbs.

NOTES

1. Structural steel for web and flange plates shall conform to ASTM designation A572 Grade 50. All other steel shall conform to ASTM designation A36 unless otherwise noted.
2. All bolts to connect cross frames to be 7/8" High Strength Bolts of ASTM designation A325.
3. After flange to web welds have been completed and before any bearing stiffeners or cross frame connection plates are welded to the web, the beams shall be inspected, and shall be in accordance with American Welding Society Specifications for Welded Highway and Railway Bridges, D2.0-69 Art. 305.
4. A maximum of two (2) transverse shop butt weld splices shall be permitted to fabricate the web plate. Transverse web splices shall not be nearer than 1'-0" to the flange splice. Location and details of butt weld shop splices shall be shown on shop drawings for approval by the Engineer.
5. The bearing stiffeners shall be welded to web plate to be plumb after erection. Cross frame connection plates may be plumb or normal to flange plates. Whichever method chosen shall be used throughout the entire job.
6. All beam dimensions are horizontal.
7. No splice shall be made in webs or flanges within 10 feet of the midspan point of any beam.

REFERENCES

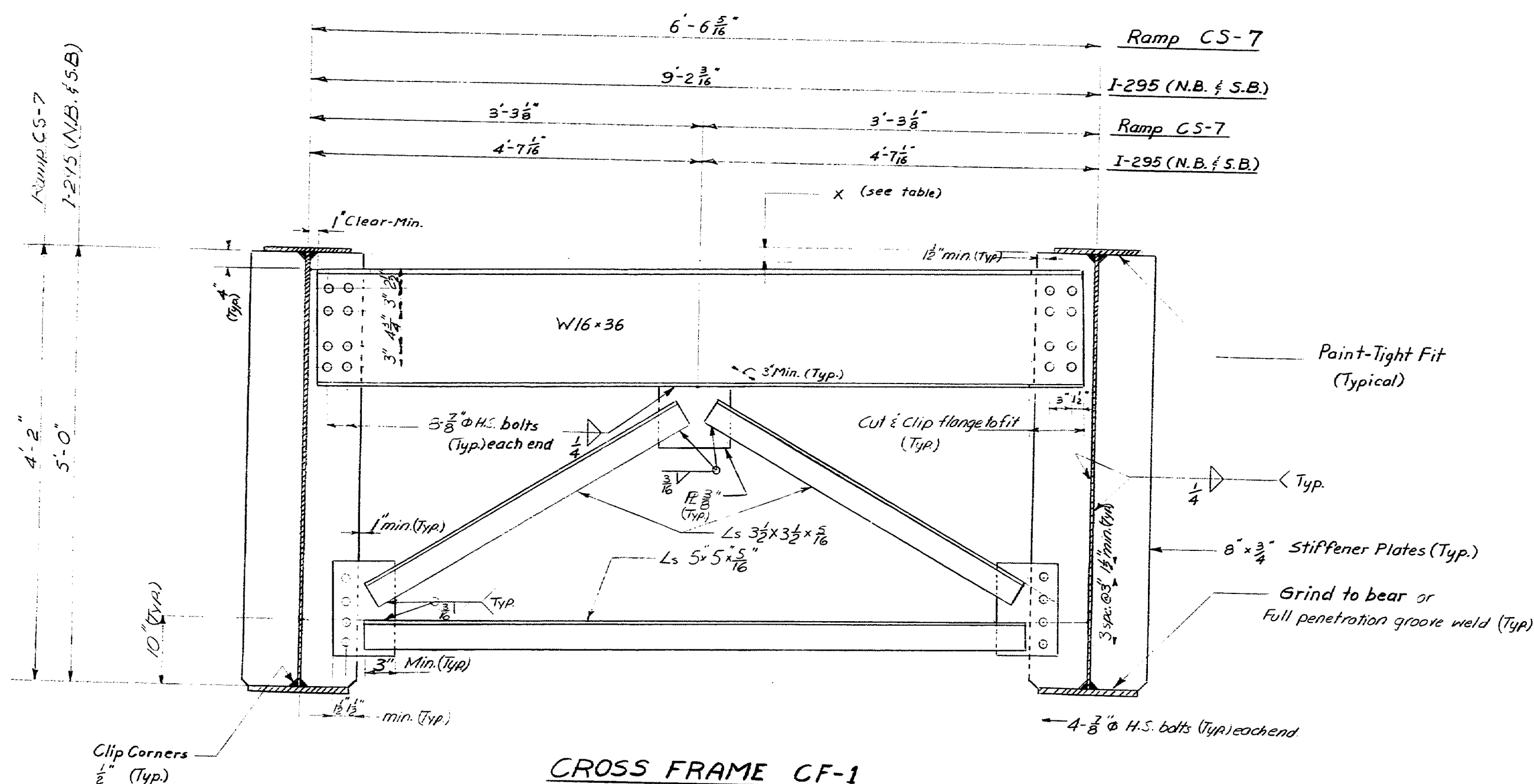
- For Framing Plan see sheet #19
For Cross Frame details see sheet #21
For Camber Diagram see sheet #22
For Shear Connector details see Standard Details-shit #BD 104-66
For Bearing Pedestal details see Standard Details-sheet #BD101-70



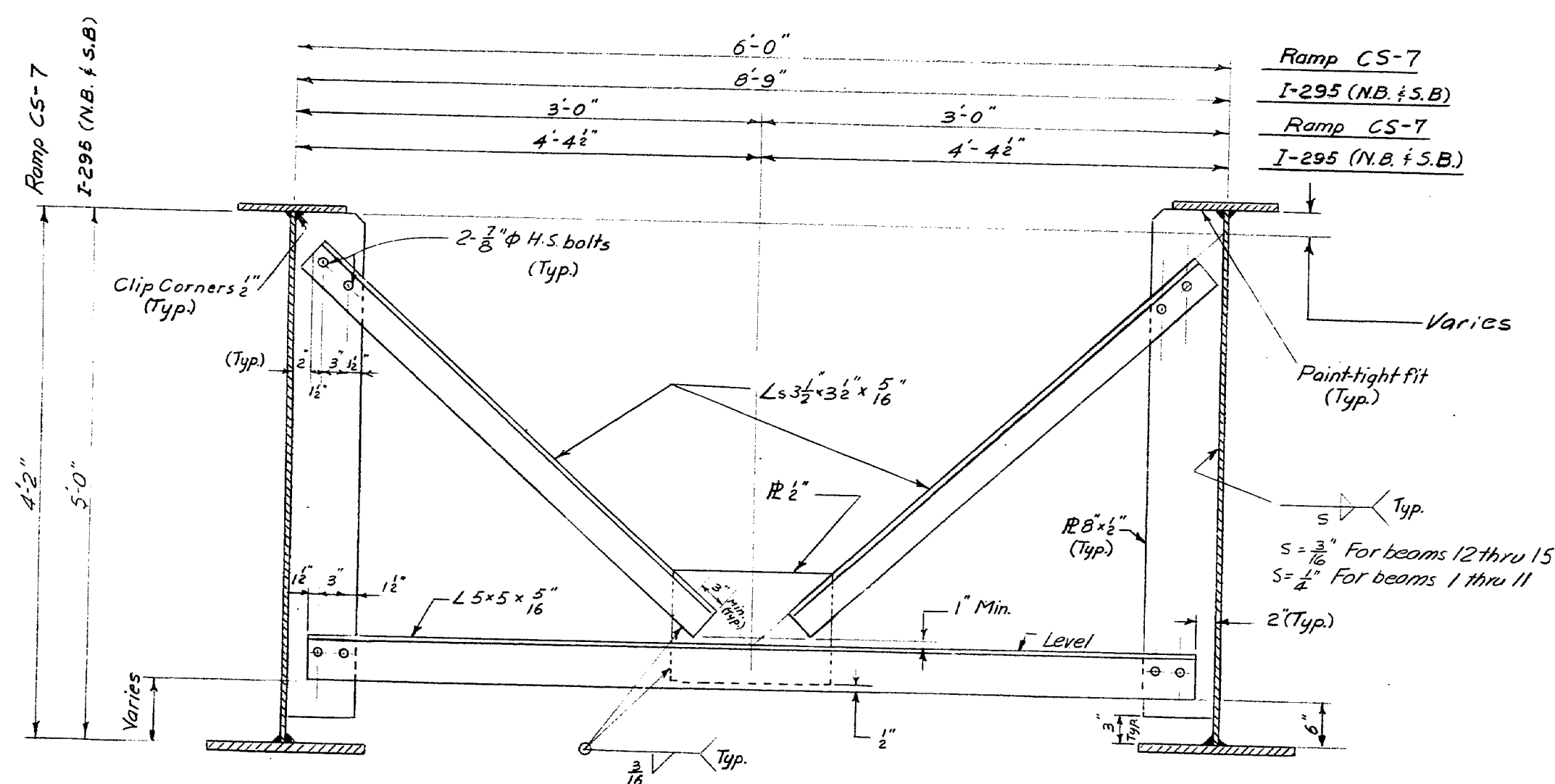
DESIGNED BY	DATE
C.F. Hall	12/1/71
CHECKED BY	DATE
M.E. R.	12/1/71
REVISIONS	
FIELD CHANGES	
PLANS	

STATE HIGHWAY COMMISSION
INTERSTATE 295 & RAMP CS-7
OVER
RELOCATED ST. JAMES STREET
IN THE CITY OF
PORTLAND
CUMBERLAND COUNTY
STRUCTURAL STEEL DETAILS
SHEET 20 OF 32 AUGUSTA, MAINE JULY 1971

153-57

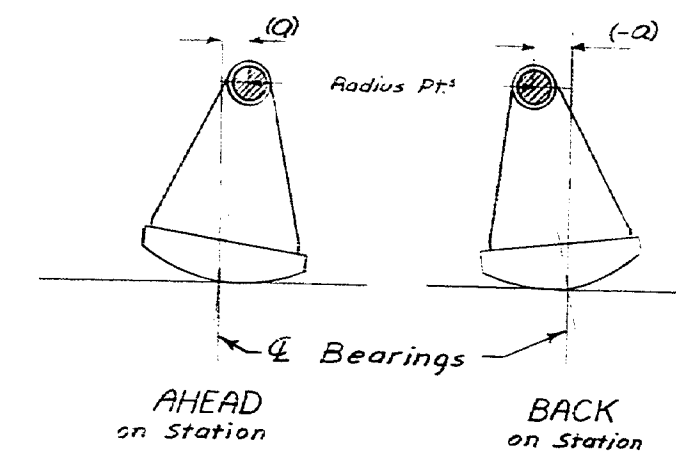


CROSS FRAME CF-1



CROSS FRAME CF-2

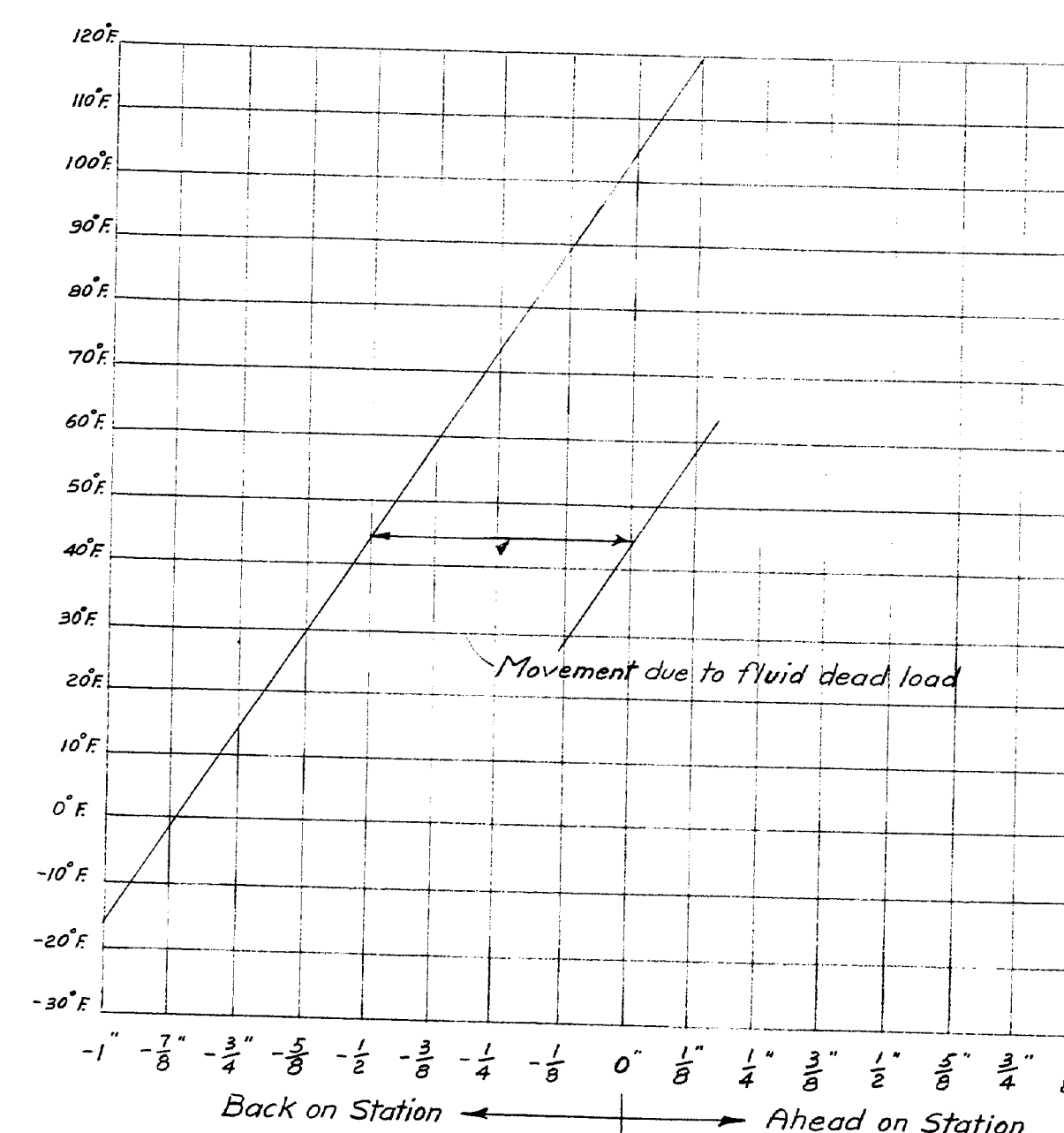
(All holes for 3/8" H.S. Bolts shall be 1/2" diameter)



ROCKERS

NOTES

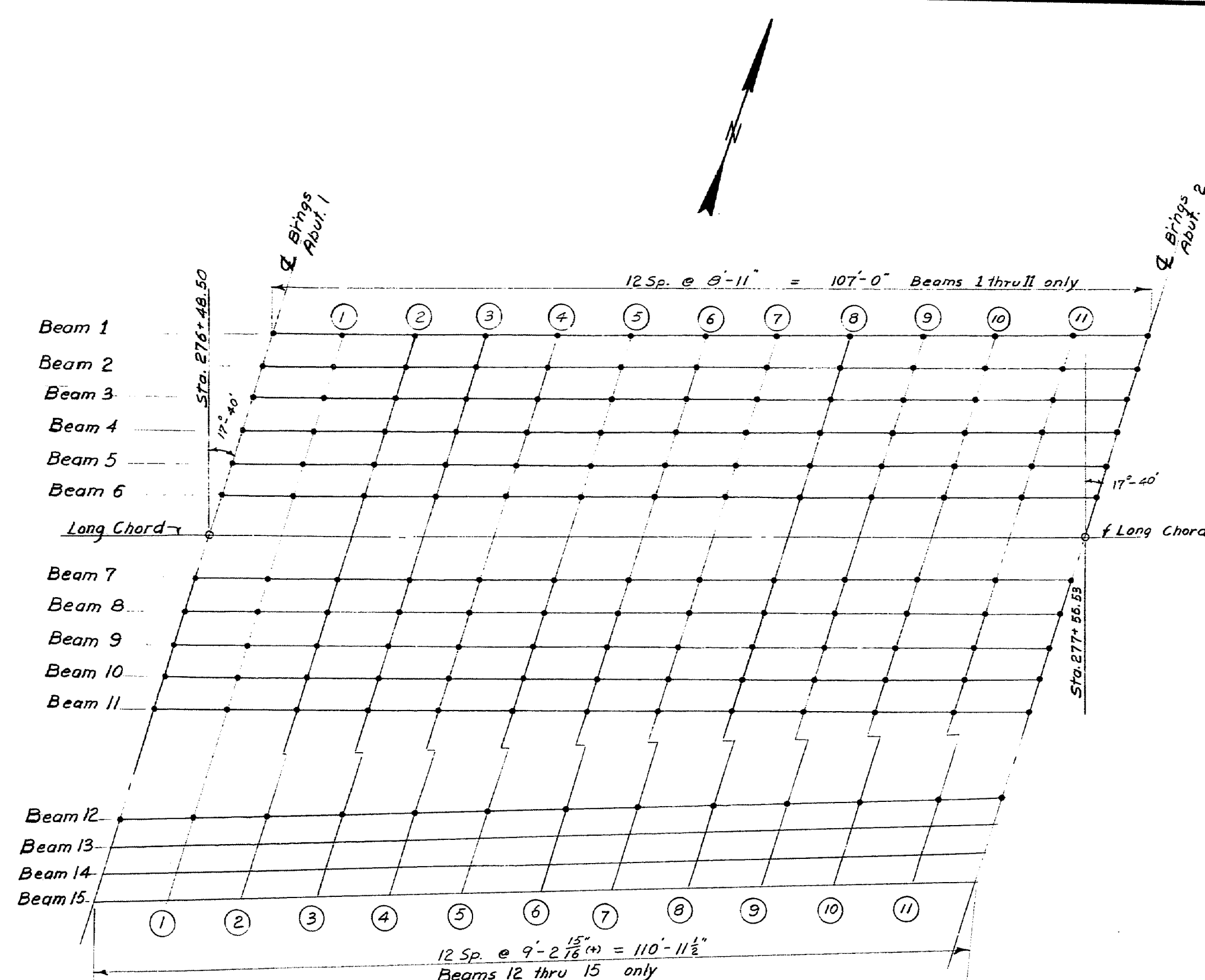
- For structural steel classification, see sheet # 20
- Expansion bearing rockers to be adjusted as given in "Rocker Setting Data" and sole plates to be welded to beam flanges after all structural steel is in place and before any formwork for the deck slab has been erected.
- For framing plan see sheet # 19
- For bearing pedestal details see "Standard Details" BD 101-70.



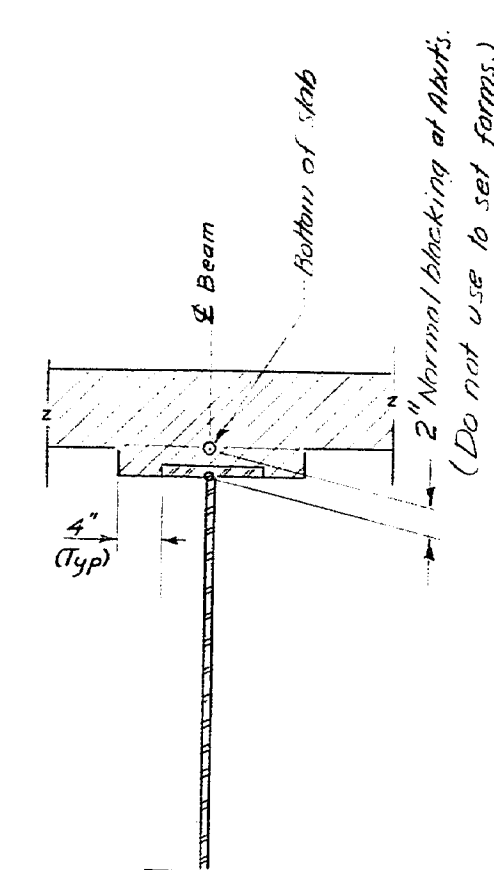
ROCKER SETTING DATA

STATE HIGHWAY COMMISSION
INTERSTATE 295 & RAMP CS-7
 OVER
RELOCATED ST. JAMES STREET
 IN THE CITY OF
PORTLAND
CUMBERLAND COUNTY
 CROSS FRAMES
 SHEET 27 OF 32 AUGUSTA, MAINE JULY 1971

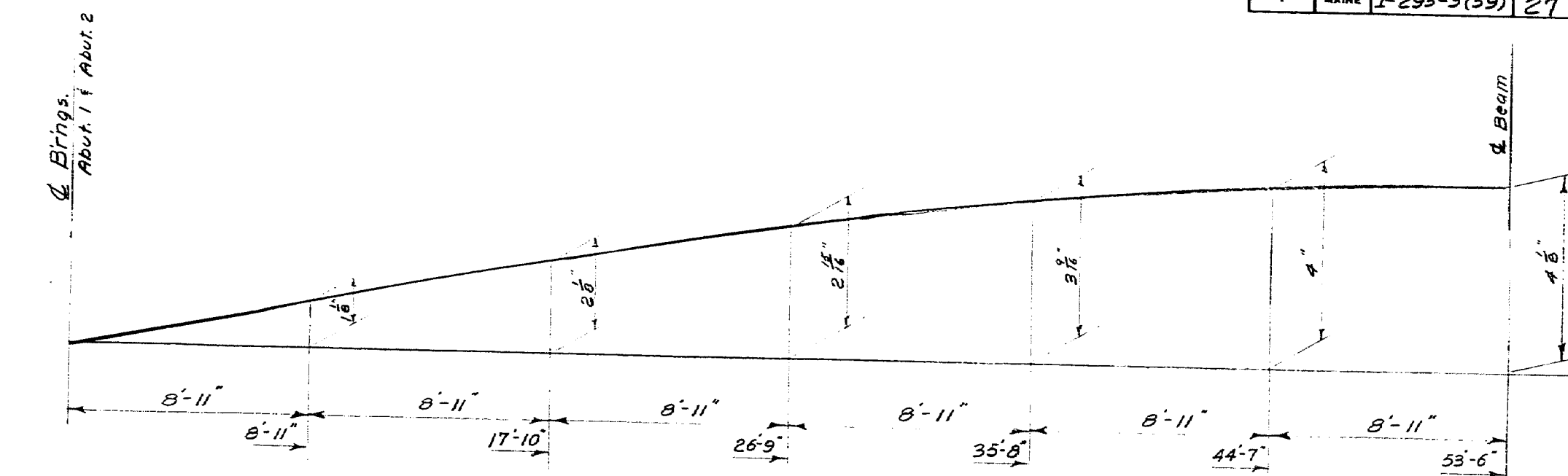
153-58



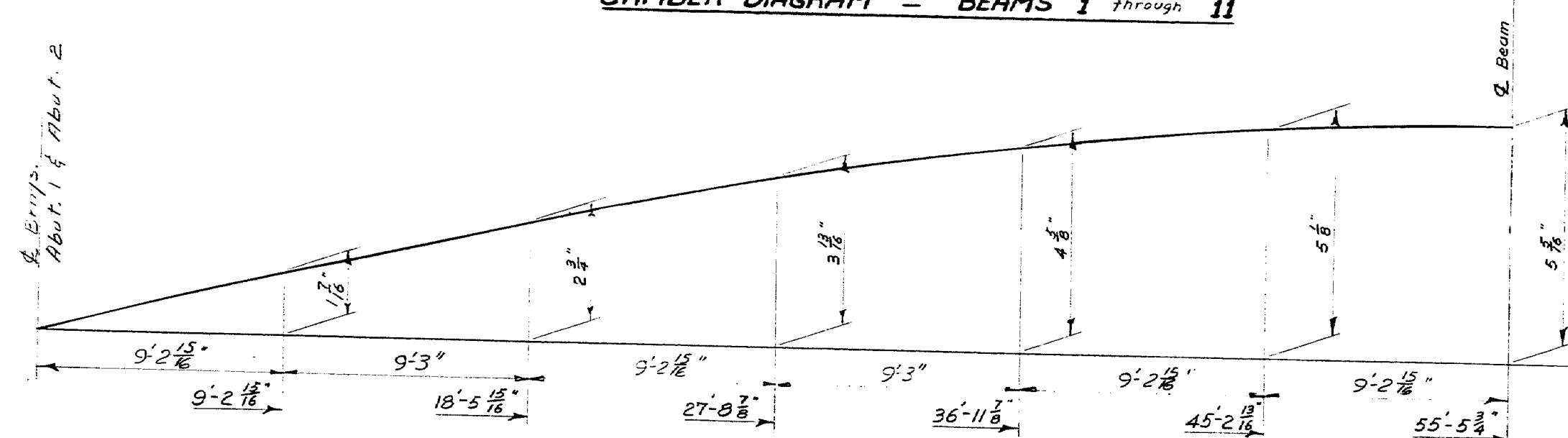
BLOCKING POINTS DIAGRAM



BLOCKING POINT DETAIL



CAMBER DIAGRAM - BEAMS 1 through 11



CAMBER DIAGRAM - BEAMS 12 through 15

Camber shown compensates for all dead load deflections
Beam deflection values will be furnished by the Engineer if required.

Notes:

1. Bottom of slab elevations in the table includes dead load deflections.
2. Before these elevations are used in setting forms, the welding of shear connectors to the top flange and bolting of the cross frames to the connection plates must be completed. No slab formwork shall be started before blocking elevations are taken.

	BOTTOM OF SLAB ELEVATIONS												
	Abut. 1	1	2	3	4	5	6	7	8	9	10	11	Abut. 2
BEAM 1	61.30	61.44	61.58	61.71	61.82	61.92	62.00	62.07	62.11	62.14	62.16	62.17	62.17
BEAM 2	61.09	61.24	61.38	61.51	61.62	61.72	61.80	61.86	61.91	61.94	61.96	61.97	61.97
BEAM 3	60.86	61.00	61.14	61.26	61.38	61.48	61.56	61.62	61.67	61.71	61.73	61.74	61.75
BEAM 4	60.47	60.61	60.75	60.88	60.99	61.09	61.17	61.24	61.29	61.32	61.34	61.35	61.36
BEAM 5	60.09	60.23	60.36	60.49	60.60	60.70	60.78	60.85	60.90	60.93	60.95	60.96	60.97
BEAM 6	59.70	59.84	59.97	60.09	60.20	60.29	60.37	60.44	60.49	60.53	60.55	60.57	60.58
BEAM 7	59.92	60.06	60.19	60.32	60.43	60.53	60.61	60.68	60.73	60.76	60.78	60.79	60.79
BEAM 8	59.53	59.67	59.80	59.93	60.04	60.14	60.22	60.29	60.33	60.37	60.39	60.40	60.40
BEAM 9	59.14	59.28	59.42	59.54	59.66	59.75	59.83	59.90	59.95	59.98	60.00	60.01	60.01
BEAM 10	58.76	58.90	59.03	59.16	59.27	59.37	59.45	59.51	59.56	59.59	59.61	59.62	59.62
BEAM 11	58.37	58.51	58.64	58.77	58.88	58.97	59.05	59.12	59.16	59.20	59.22	59.23	59.23
BEAM 12	44.02	44.50	44.97	45.42	45.86	46.28	46.68	47.06	47.42	47.76	48.07	48.33	48.67
BEAM 13	43.78	44.25	44.72	45.17	45.61	46.03	46.43	46.81	47.17	47.51	47.83	48.14	48.45
BEAM 14	43.54	44.01	44.48	44.94	45.37	45.79	46.20	46.58	46.94	47.28	47.60	47.91	48.22
BEAM 15	43.30	43.77	44.24	44.70	45.14	45.56	45.96	46.34	46.70	47.04	47.37	47.68	47.99

Raised grade 0.04' per C.O. #15

Raised grade 0.08' per C.O. #15

3/10/74

DATE	BY	REVISION	FIELD CHANGES
7-71	CJ		
11-72	W.E.C.		
11-72	W.E.C.		

PLANS

STATE HIGHWAY COMMISSION
INTERSTATE 295 & RAMP CS-7
 OVER
RELOCATED ST. JAMES STREET
 IN THE CITY OF
PORTLAND
CUMBERLAND COUNTY
 BOTTOM OF SLAB ELEVATIONS
 CAMBER DIAGRAM
 SHEET 22 OF 32 AUGUSTA, MAINE JULY 1971

153-59

PLAN - SUPERSTRUCTURE SLAB - S.B.

Chord Length L. Fascia = 106'-5"

Chord Length R. Fascia = 106'-11 1/2"

107'-0" c/c Bearings

STA. 276+48.5

STA. 277+55.53

FASCIA LAYOUT

Left Fascia

Right Fascia

Varies

24'-0"

5'-5"

7'-1 1/4" Radial

DETAIL C

2" Wearing Surface

1/2" Membrane Waterproofing

1" V-Groove

1/2" drip notch (Typ)

5 Spaces @ 8'-9" = 43'-9"

TRANSVERSE SECTION

Back of Backwall

Top of Curb

2-Bar Aluminum Bridge Rail

1" V-Groove

1/2" drip notch (Typ)

Use form oil to break bond. Shall have a non-shrink additive. See note 11, sheet 25 of 32.

END POST DETAIL

Left End Post shown

Right End Post similar

SUPERSTRUCTURE NOTES

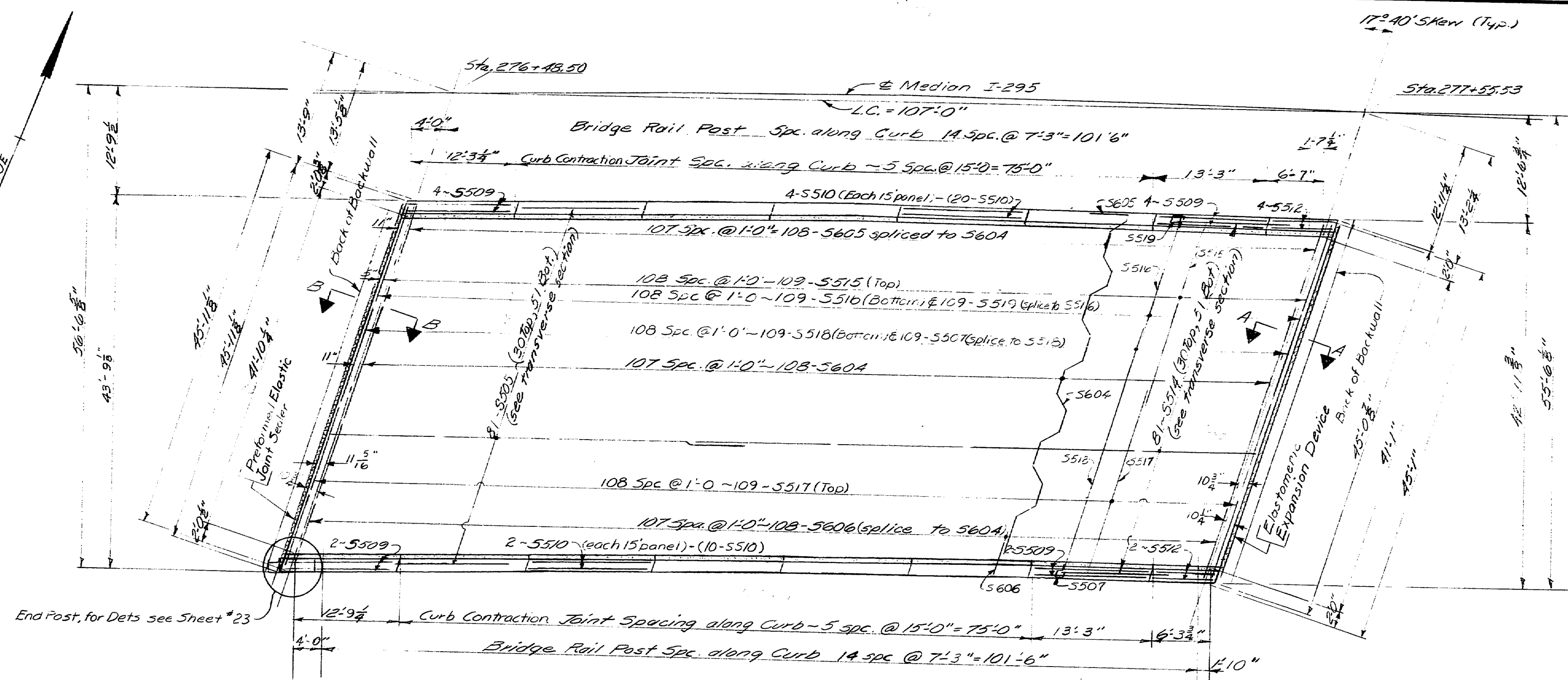
1. All reinforcing steel to have 2" clearance and a minimum of 36 bar diameter splice except as noted.
2. Break the bond at all curb contraction joints by a method to be approved by the Engineer. Provide joint in vertical bridge curb at all curb contraction joints.
3. All exposed surfaces of End Posts, top and vertical sides of curbs down to the 1/2" drip notch shall be coated with "Protective Coating for Concrete Surfaces" Item 515.20.
4. Drip tubes shall be placed at 10' intervals along the low side of slabs as shown in Detail "C" below. Payment incidental to Item 502.26
5. Payment for concrete in the End Posts on the superstructure slab shall be included in Pay Item 502.26 Structural Concrete, Roadway & Side-walk Slabs on Steel Bridges.
6. Chamber all exposed edges of concrete 1/2".

REFERENCES:

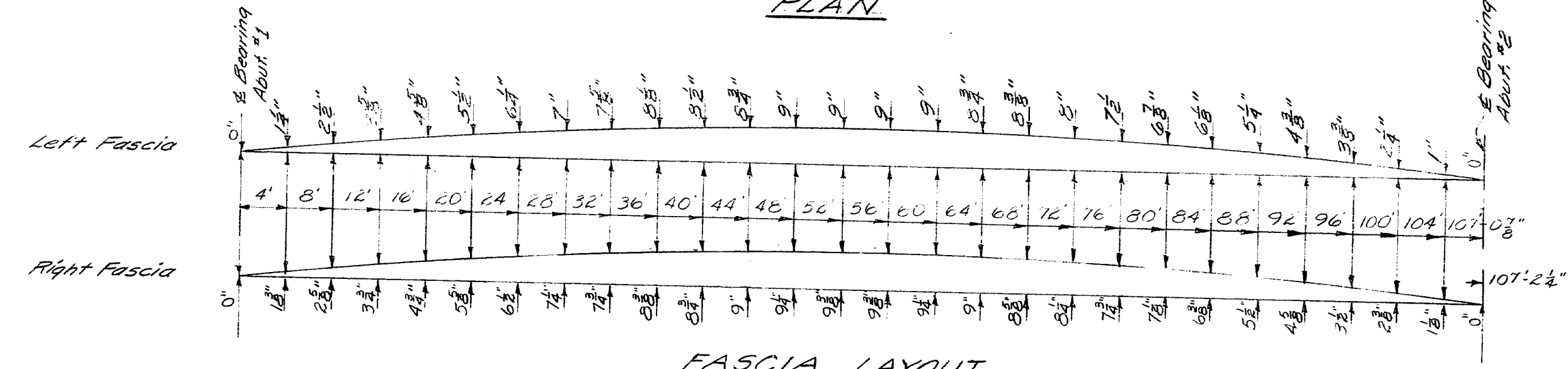
1. For Reinforcing Steel see sheet #32
2. For Blocking Points and Bottom of Slab Elevations see sheet #22
3. For details of Bridge Rail see Standard Details sheet # BD 106-69
4. For sections A-A & B-B see Sheet #24
5. For 1" V-Groove Detail see Sheet #24
6. For details of Vertical Bridge Curb Type see sheet #24
7. 3/4" Conduit and Fittings to be paid for under Item 638.01
8. See sheet 30 of 32 for Conduit Details
9. Mortar for concrete curb shall have a non-shrink additive. See note 11, sheet 25 of 32

203-1 20

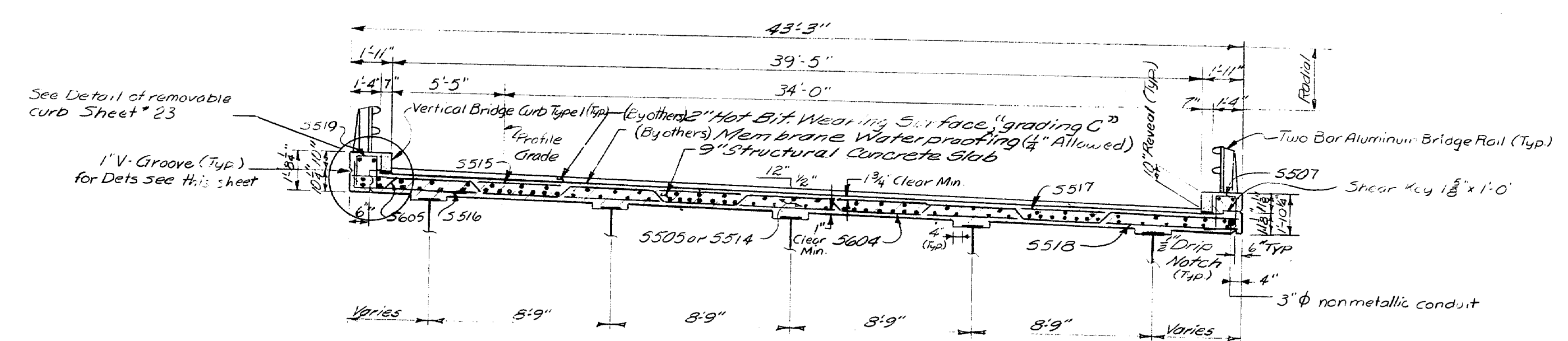
R.P.R. SHEET NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	I-295-3 (59)	29	84



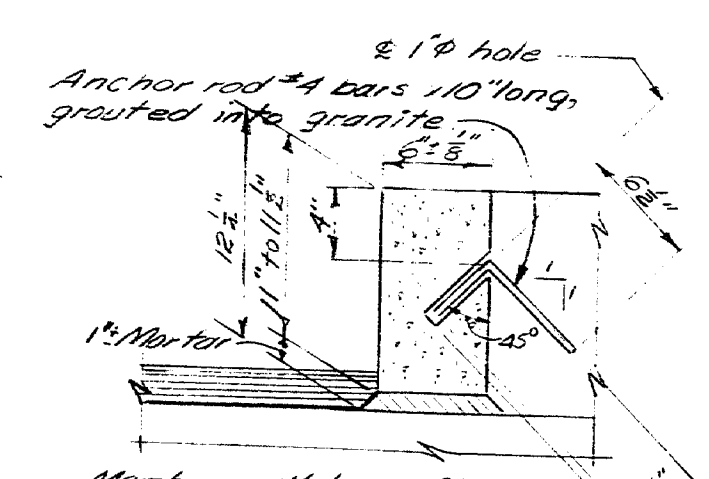
PLAN



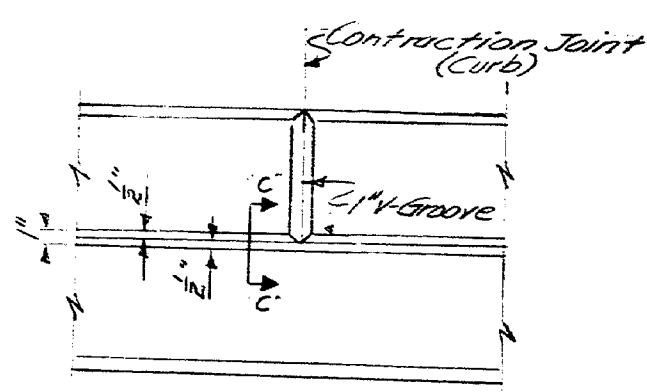
FASCIA LAYOUT



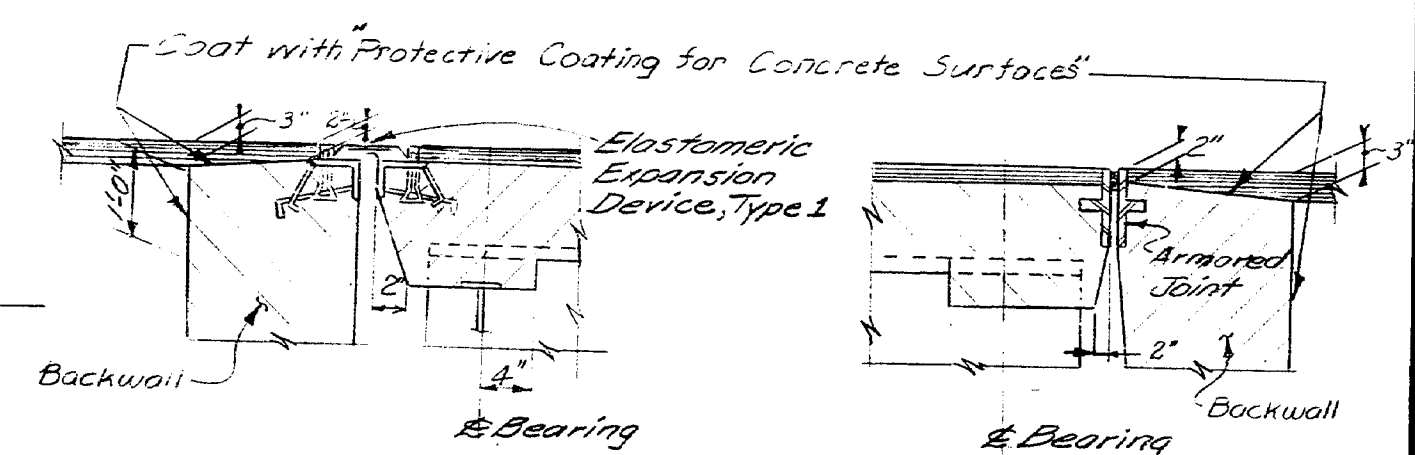
TRANSVERSE SECTION



VERTICAL BRIDGE CURB TYPE 1



1/4 V-GROOVE DETAIL



SECTION A-A

SECTION B-B

NOTES:
 For Notes: See Ramp C-5, 7 & S-B, Sheet # 23 & 25
 For Removable Curb Detail: See Sheet # 23
 For Conduit Details: See Sht. # 30

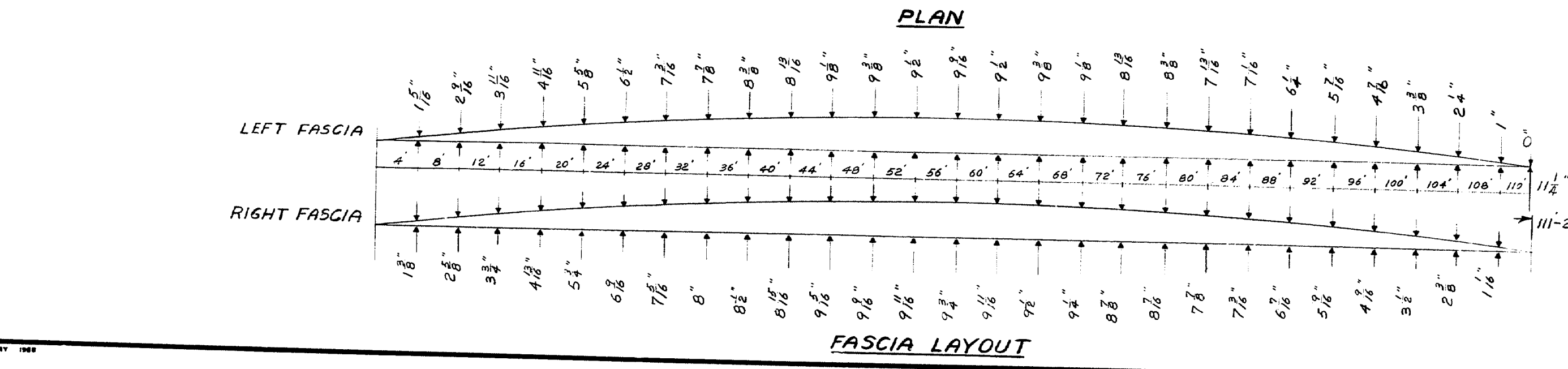
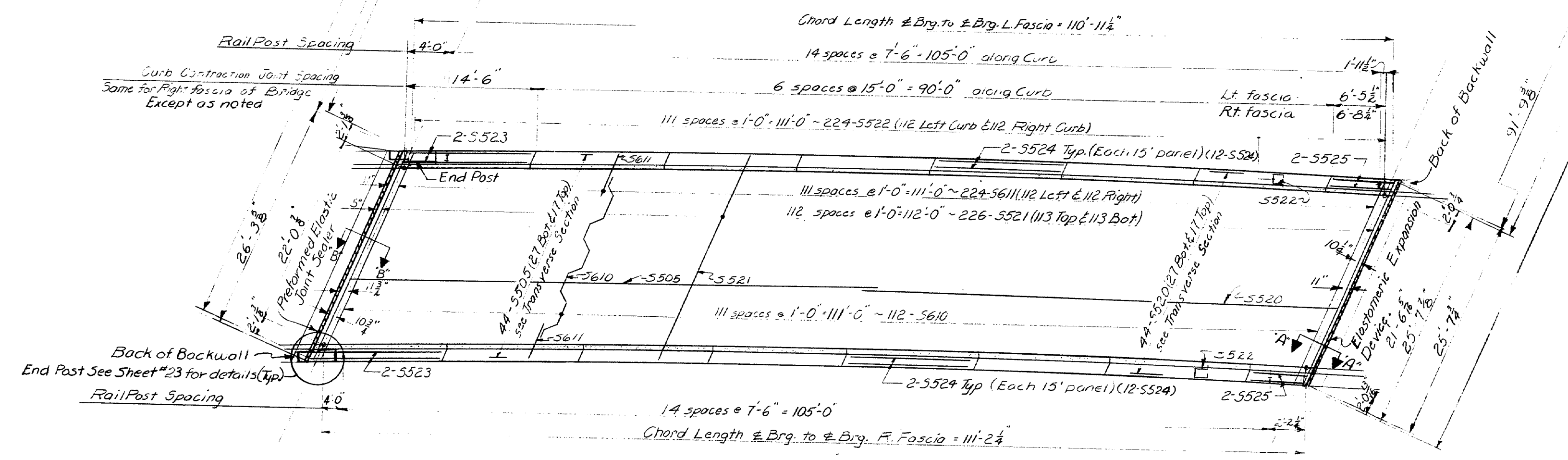
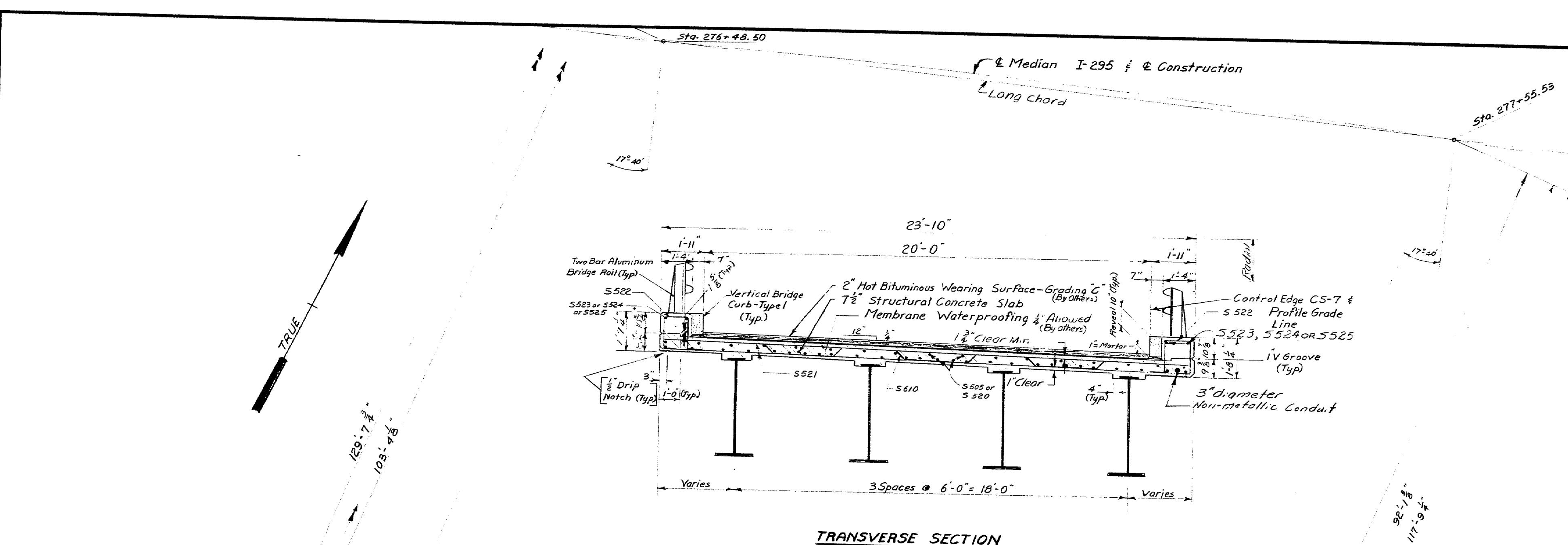
STATE HIGHWAY COMMISSION
INTERSTATE 295 & RAMP CS-7
 OVER
RELOCATED ST. JAMES STREET
 IN THE CITY OF
PORTLAND
 CUMBERLAND COUNTY
 SUPERSTRUCTURE - NORTHBOUND
 SHEET 24 OF 32 AUGUSTA, MAINE JULY 1971

153-61

DESIGNED BY	CHECKED BY	DATE
W.E.A.	W.E.A.	8-77
REVISIONS		
FIELD CHANGES		

PLANS

S. P. R. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	I-295-3 (39)	30	34

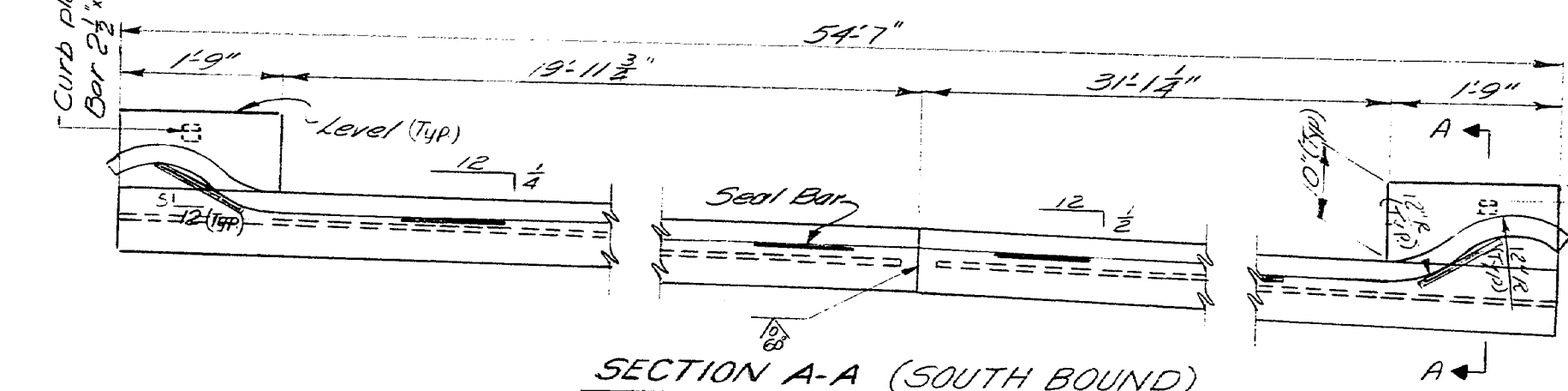
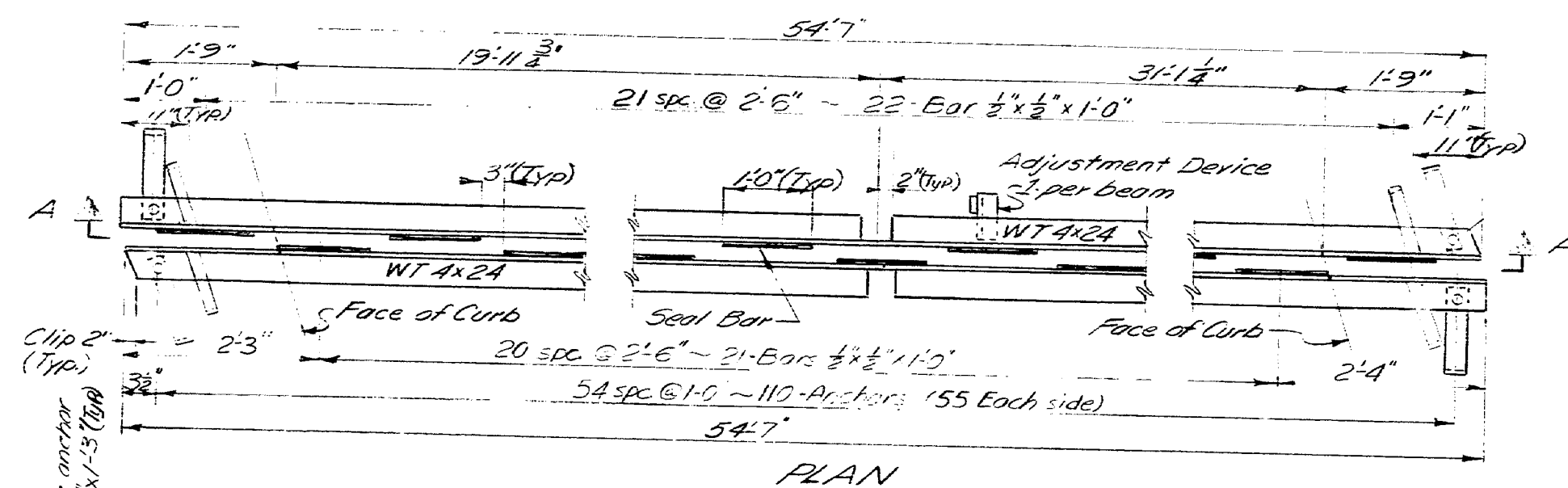


- NOTES:**
1. For General Superstructure Notes see sheet 23
 2. For Armored Joint details see sheet 26
 3. For Reinforcing Steel see sheet 32
 4. For Bottom of Slab Elevations and Blocking Points details see sheet 22
 5. For Details of Bridge rail see Standard Details BD 106-69
 6. For sections A-A & B-B see sheet 24
 7. For 1"-V Groove Detail see sheet 24
 8. For Details of Vertical Bridge Curb-Type I see sheet 24
 9. For Elastomeric Device details see sheet 27
 10. For 3" Conduit details see sheet 30 of 32
 11. Mortar for bedding and jointing granite curb shall have a nonshrink additive which shall be subject to the approval of the Engineer.

STATE HIGHWAY COMMISSION
INTERSTATE 295 & RAMP CS-7
 OVER
RELOCATED ST. JAMES STREET
 IN THE CITY OF
PORTLAND
CUMBERLAND COUNTY
 SUPERSTRUCTURE SLAB
 RAMP CS-7
 SHEET 25 OF 32 AUGUSTA, MAINE JULY 1971

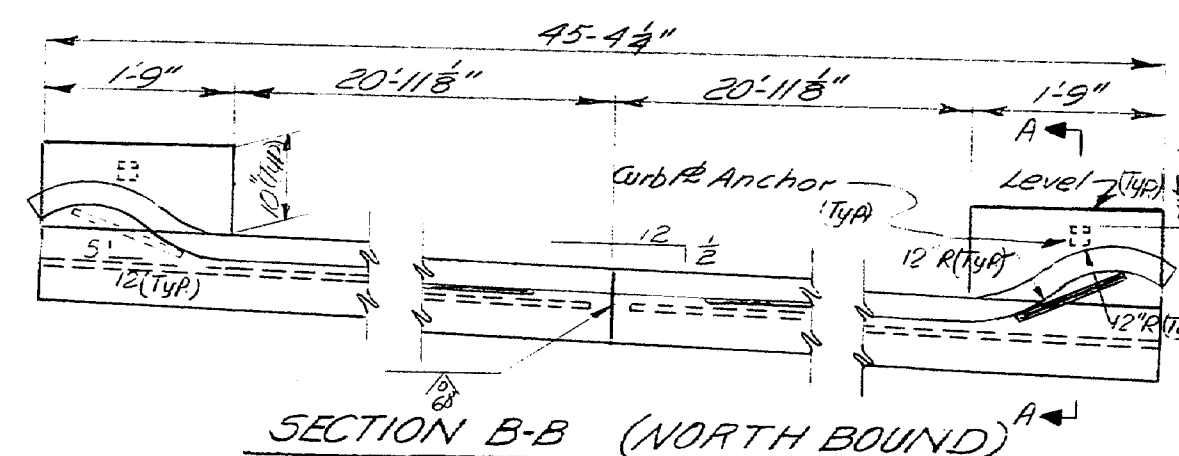
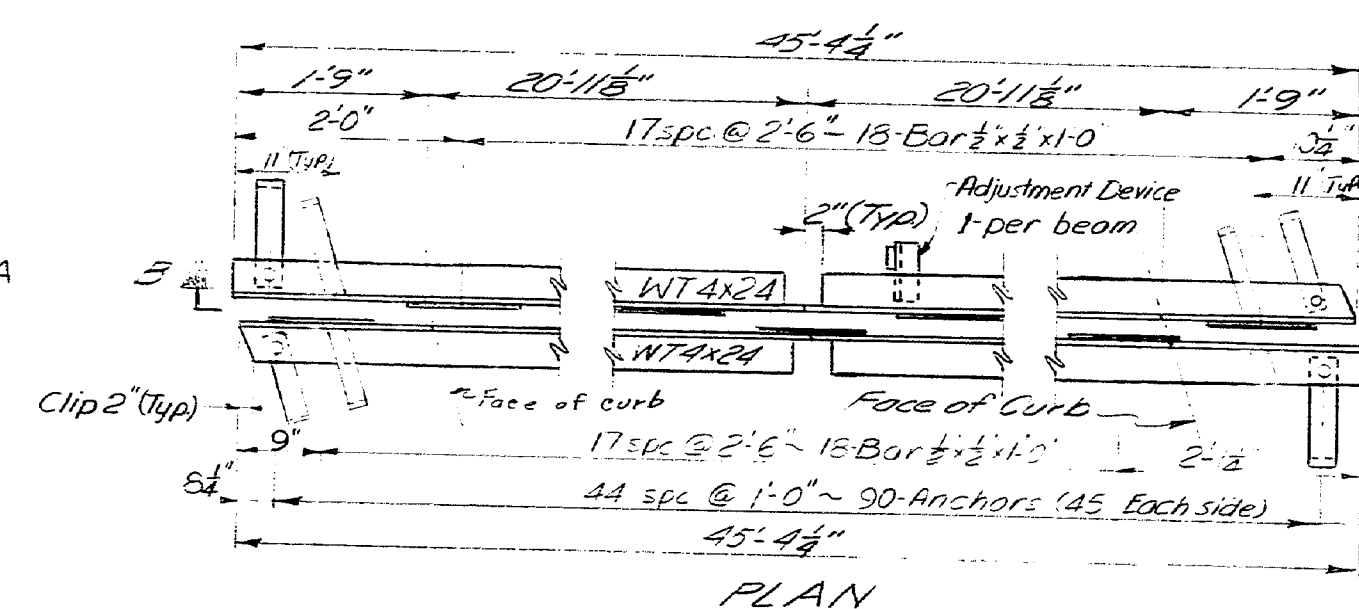
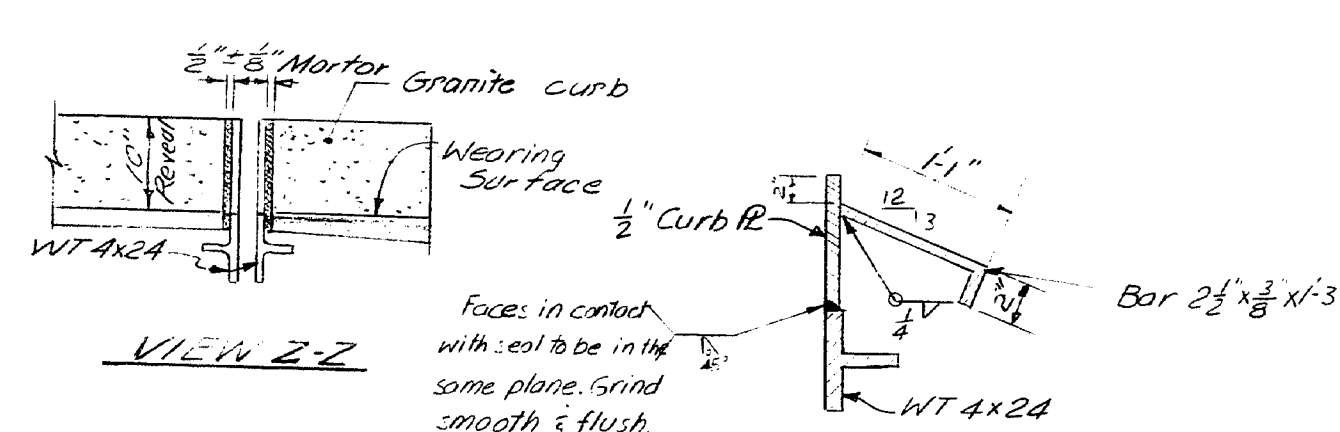
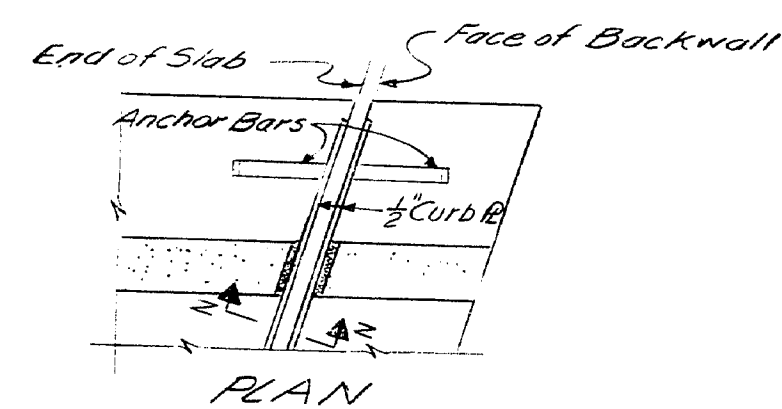
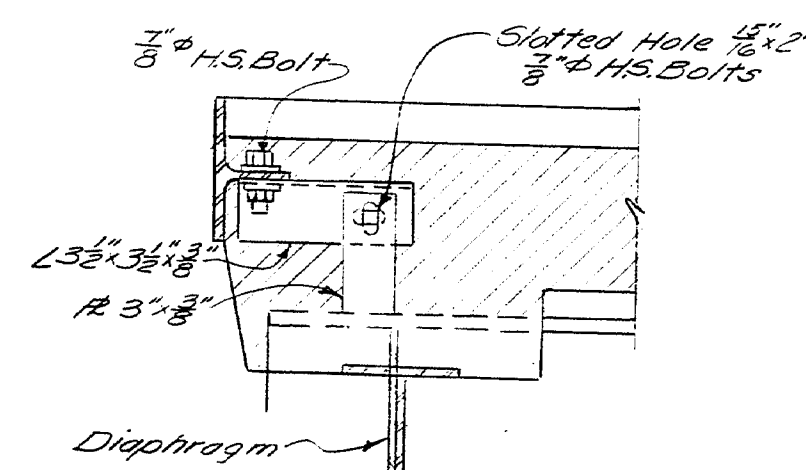
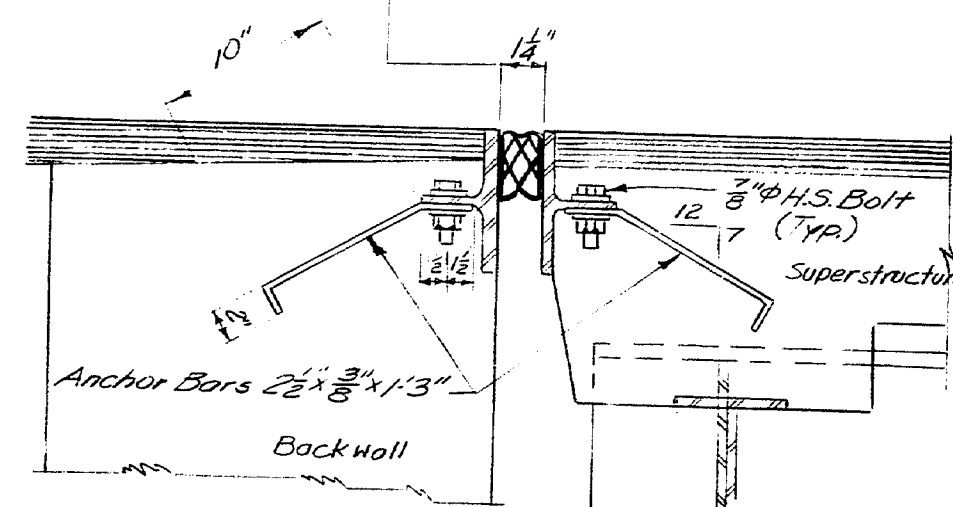
153-62

B. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOT SHE
1	MAINE	I-295-3(59)	31	8

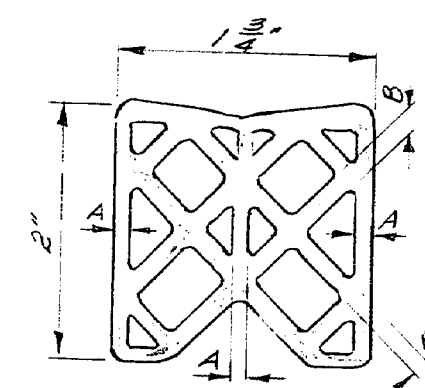
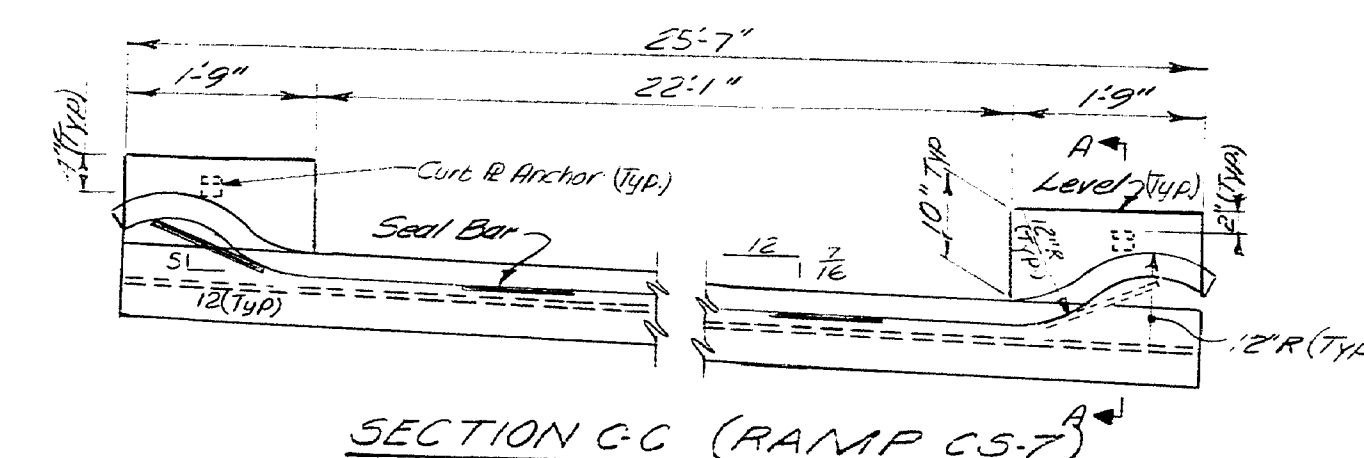
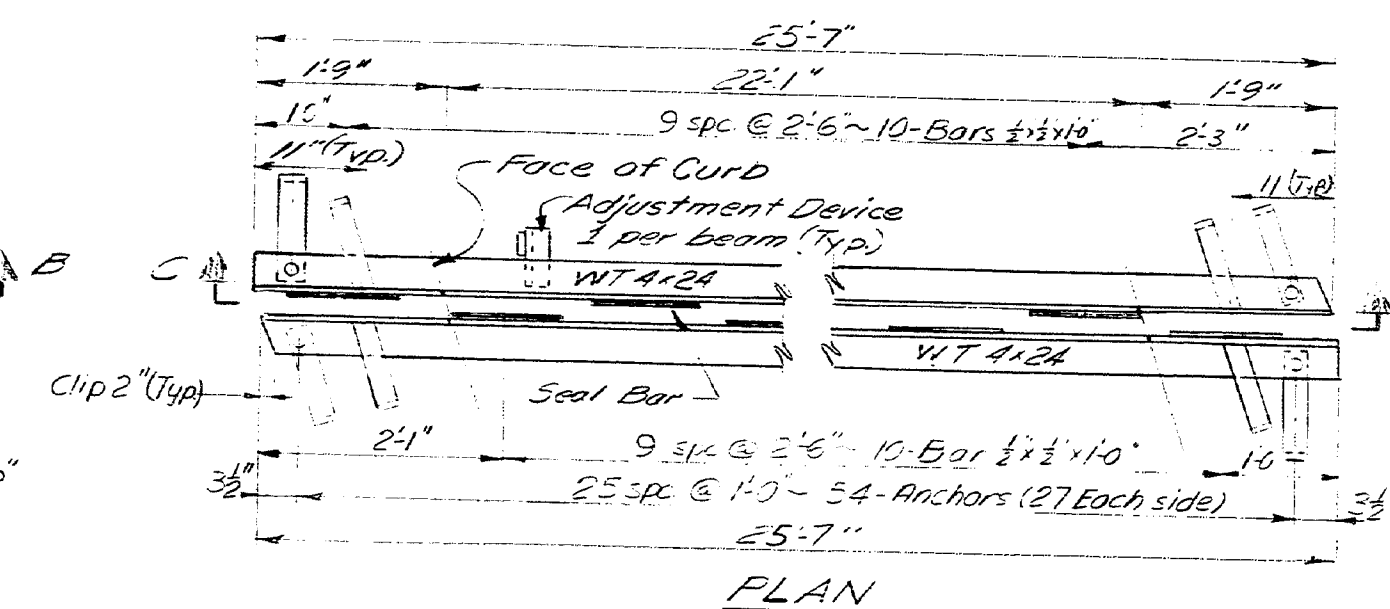


NOTE

NOTE:
At the time the armored joint is installed at abutment #1 end of superstructure, place on 1" gap to allow for movement (3" anticipated) which shall occur when the superstructure concrete is placed. No adjustment for placing concrete is necessary at abutment #2 (expansion end) except for temperature.

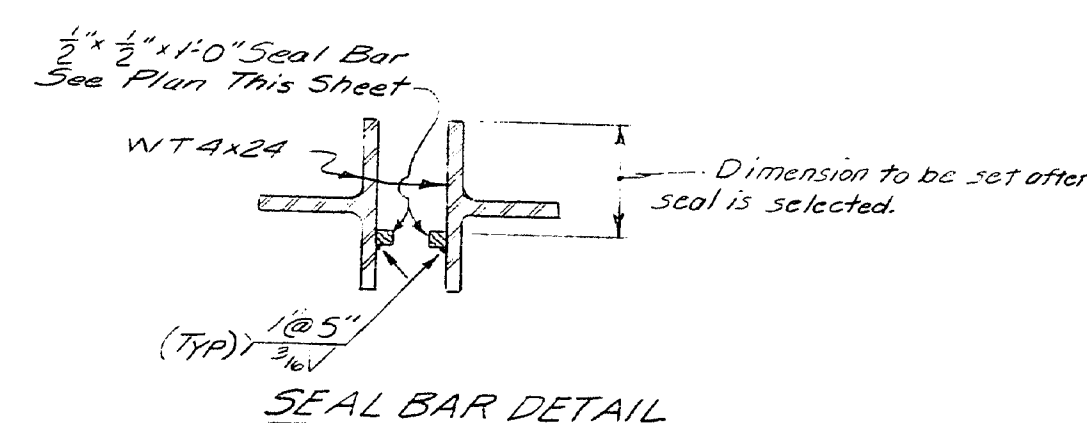


ARMORED JOINT DETAILS ABUTMENT NO.1



PREFORMED ELASTIC JOINT SEALER

The configuration of the Preformed Elastic Joint Sealer may be changed from that shown in order to conform with shapes as produced by various manufacturers. However, the cross-sectional (A&B) dimensions, including those of the internal elements & the shell (A&B) shall be approved by the Engineer before ordering the Preformed Elastic Joint Sealer.

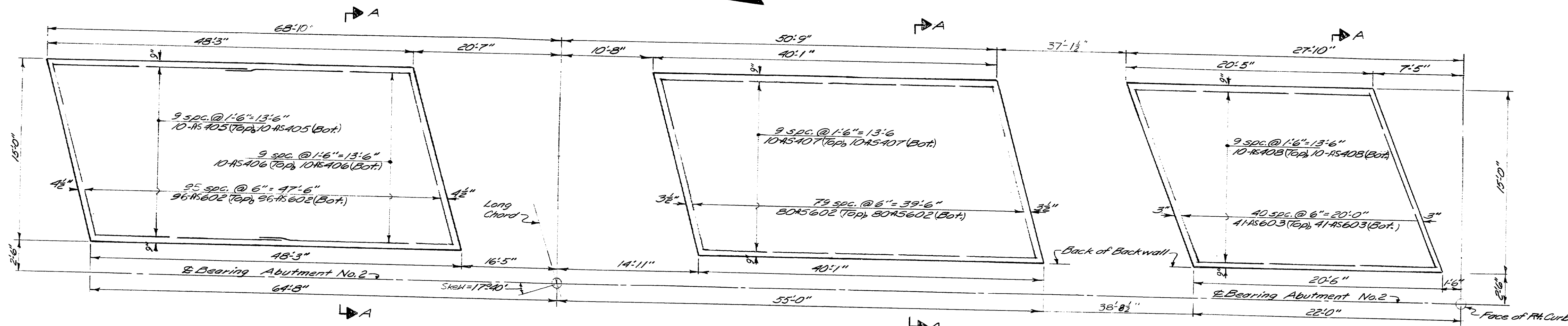


PLANS	By <i>W. S. Eng. All.</i>		BY	DATE
	DESIGN - DETAILED		<i>MEH</i>	<i>4-5-71</i>
	CHECKED			
	REVISIONS			
	FILED		FILED	

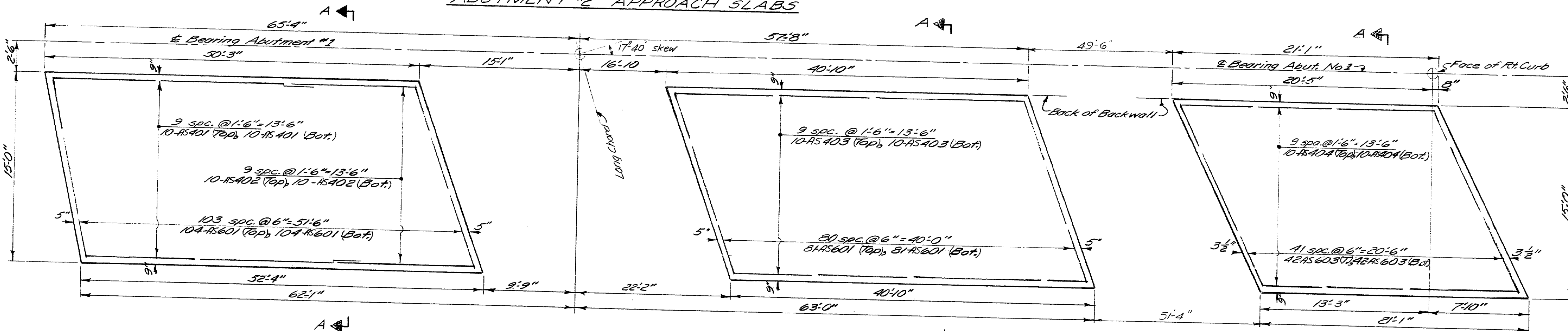
STATE HIGHWAY COMMISSION
INTERSTATE 295 & RAMP CS-7
OVER
RELOCATED ST. JAMES STREET
IN THE CITY OF
PORTLAND
CUMBERLAND COUNTY
ARMORED JOINT — ABUT. I
SHEET 24 OF 32 AUGUSTA, MAINE JULY 1971

SHEET 26 OF 32 AUGUSTA, MAINE JULY 1971

153-63



PLAN
ABUTMENT #2 APPROACH SLABS

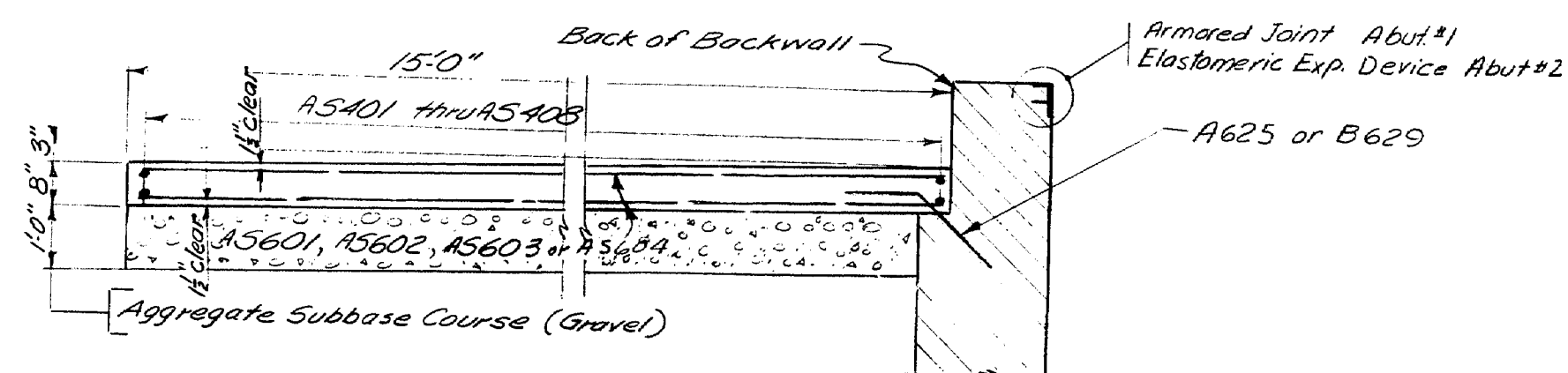


SOUTH BOUND

PLAN
ABUTMENT #1 APPROACH SLABS

NORTH BOUND

RAMP CS-7



SECTION A-A

REINFORCING STEEL SCHEDULE-APPROACH SLABS				
STRAIGHT BARS				
Mark	Size	Number	Length	Location
AS401	#4	20	40'0"	At Abutment No. 1 (S.B.)
AS402	#4	20	13'6"	No. 1 (S.B.)
AS403	#4	20	40'0"	No. 1 (N.B.)
AS404	#4	20	20'9"	No. 1 (Ramp)
AS405	#4	20	40'0"	No. 2 (S.B.)
AS406	#4	20	9'0"	No. 2 (S.B.)
AS407	#4	20	39'9"	No. 2 (N.B.)
AS408	#4	20	20'0"	At Abutment No. 2 (Ramp)
AS601	#6	370	15'6"	At Abutment No. 1 (N.B. & S.B.)
AS602	#6	352	15'0"	No. 2 (N.B. & S.B.)
AS603	#6	82	15'9"	No. 2 (Ramp)
AS604	#6	84	16'3"	At Abutment No. 1 (Ramp)

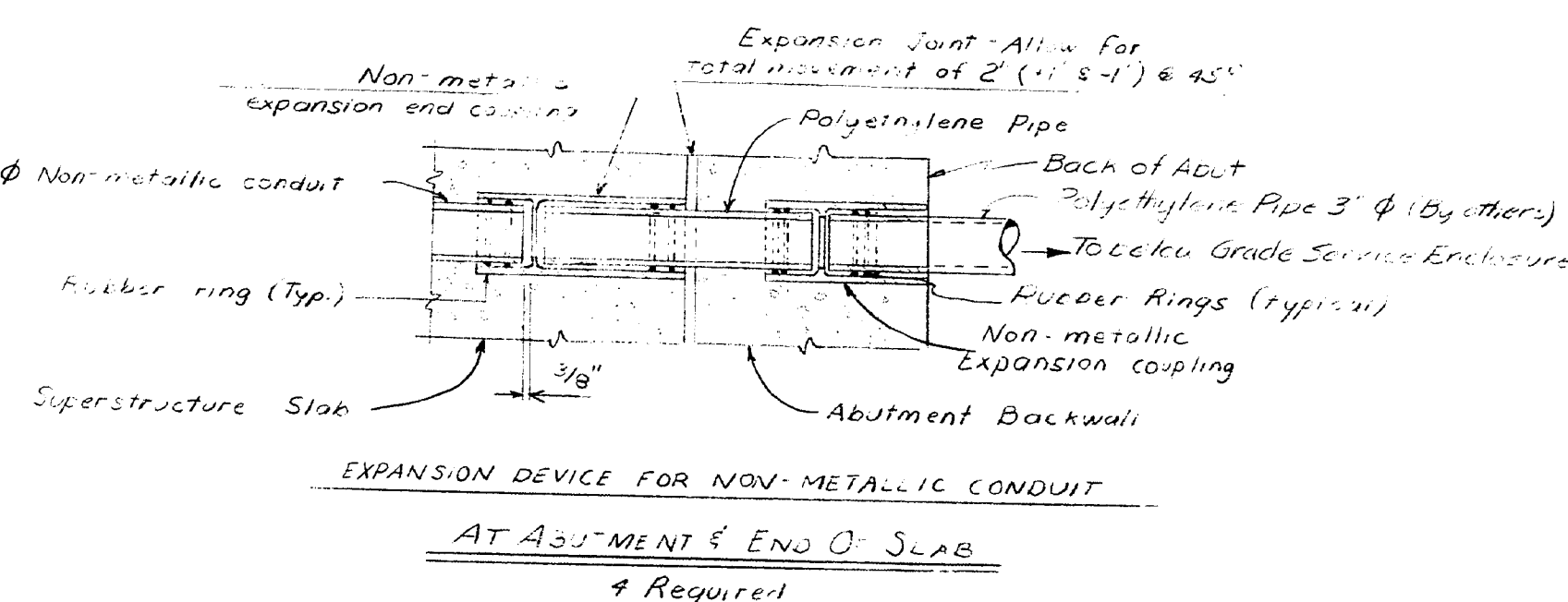
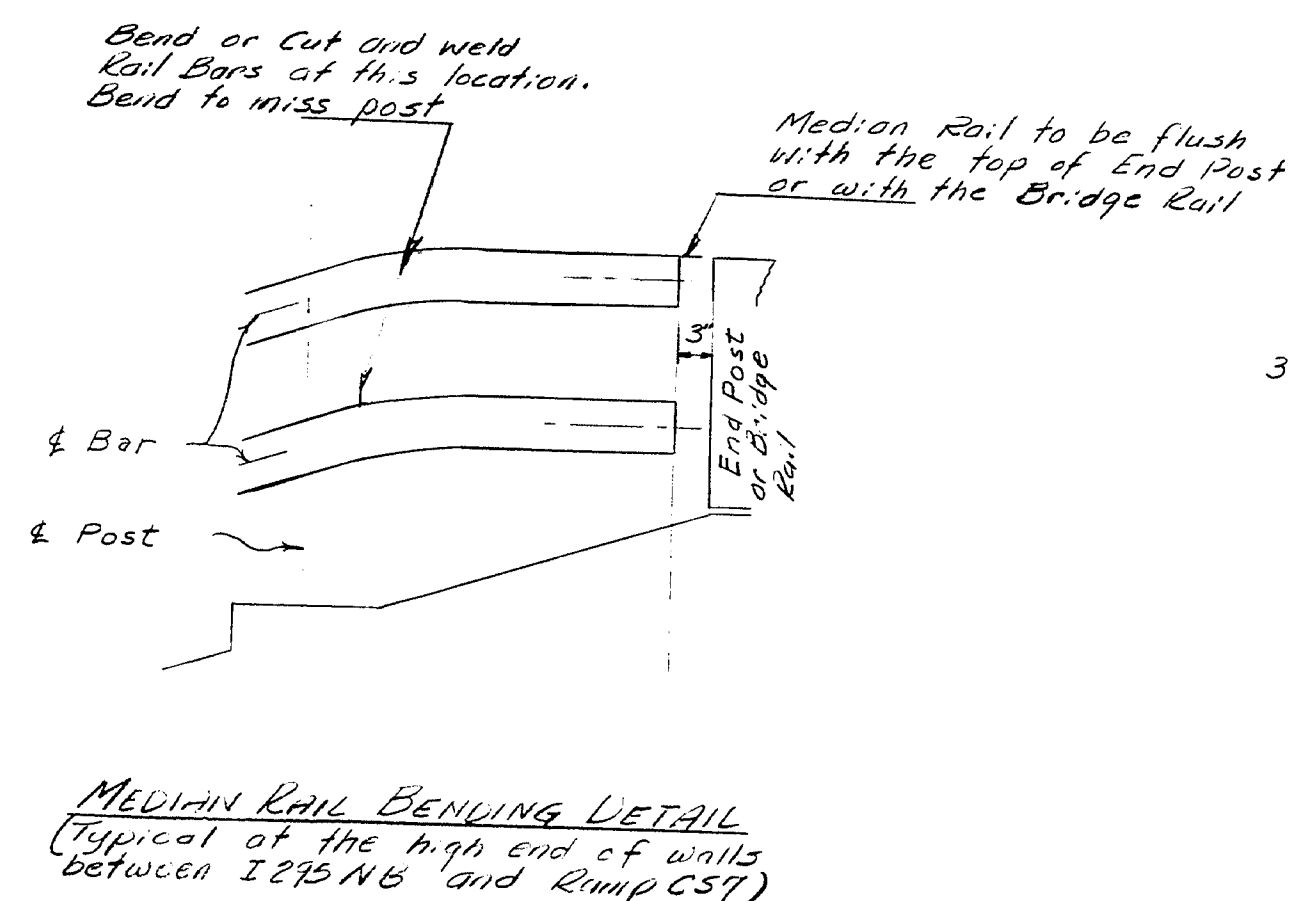
Note
Approach Slabs to be constructed
by others.

STATE HIGHWAY COMMISSION
INTERSTATE 295 & RAMP CS-7
OVER
RELOCATED ST. JAMES STREET
IN THE CITY OF
PORTLAND
CUMBERLAND COUNTY
APPROACH SLABS

SHEET 29 OF 32 AUGUSTA, MAINE JULY 1971

153-66

PLANS	By: <i>Lee, Exp: All</i>		DATE
	DESIGN - DETAILED	MM/6	1/72
	CHECKED	N.E.A.	
	REVISIONS		
	FIELD CHANGES		

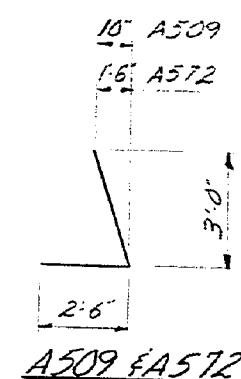


STATE HIGHWAY COMMISSION
INTERSTATE 295 & RAMP CS-7
OVER
RELOCATED ST. JAMES STREET
IN THE CITY OF
PORTLAND
CUMBERLAND COUNTY
BRIDGE RAIL ALONG ABUTMENTS
SHEET 30 OF 32 AUGUSTA, MAINE JULY 1991

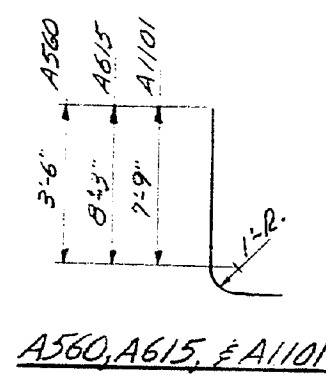
SHEET 30 OF 32 AUGUSTA, MAINE July 192

153-67

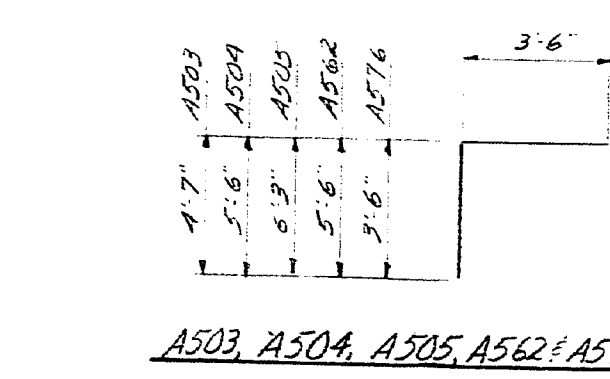
ABUTMENT No. 1



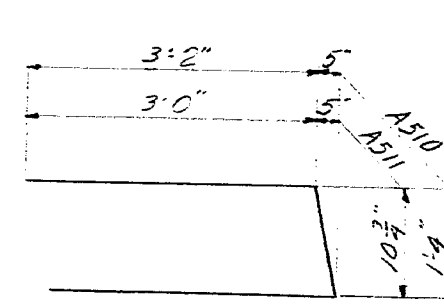
A401, A402, A514, A528,
A532, A567 & A575



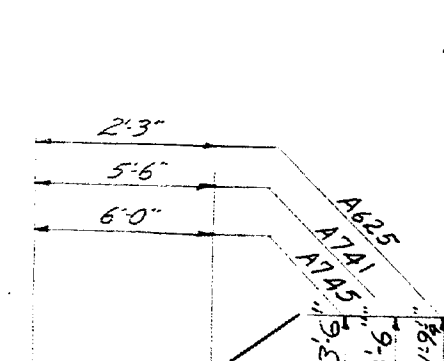
A570 & A571



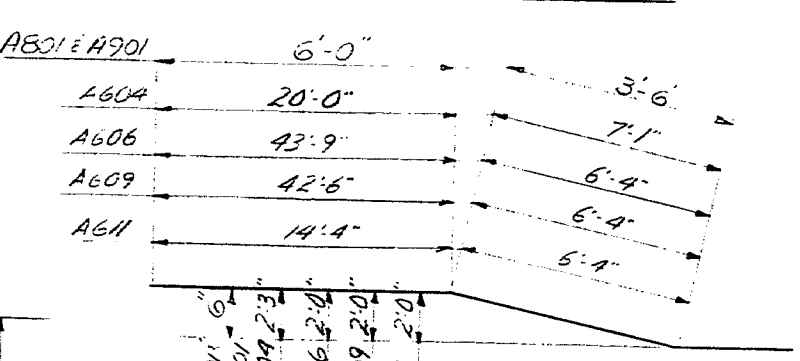
A503, A504, A505, A562 & A576



A510 & A511



A625, A741 & A745



A604, A606, A609, & A611 A801 & A901

	Map	Sec	No.	Length	Location
	A604	8	11	29'4"	Footling (Median)
2)	A606	1	11	32'4"	Footling (N.B.)
	A609	8	51'1"	Footling (N.B. & Ret. Wall)	
UNB611	A611	7	22'11"	Footling wing	
	A615	1	26	10'3"	Footling Powel wing
2)	A625	6	14	9'6"	Approach (Slo. Sect)
	A741	47	67	9'0"	Breast wall (N.B. & B. Median)
	A745	47	16	9'6"	Breast wall (N.B.)
	A901	2	29	12'3"	Wing (S.B.)
	A901	2	29	12'3"	Ret wall
	A1104	71	255	9'9"	Footling to Breastwall (Approach)

	A	I	G	H	T	
A550	*5	2	23'-6"	Ret. Wall		
A551		13	6'-6"	Wing South		
A552		8	10'-0"			
A553		8	19'-3"			
A554		2	18'-2"			
A555			15'-4"			
A556			11'-11"			
A557			8'-9"			
A558			5'-8"			
A559		2	19'-2"	Wing South		
A561		44	8'-0"	Breastwall (Romp)		
A563		2	17'-6"	Breastwall (Romp)		
A564		6	25'-6"	Breastwall (Romp)		
A565		12	20'-6"	Breastwall & Backwall (Romp)		
A566		36	5'-6"	Backwall (Romp)		
A573		10	7'-0"	Breastwall & Backwall (Romp)		
A574		4	4'-0"	Curtain Wall (Romp)		
A577		16	22'-6"	Ret. Wall		
A578		8	5'-9"	Wing South		
A579		8	8'-0"	Wing South		
A580	*5	18	3'-9"	Curtain Wall		
A601	*6	20	22'-0"	Footling - Wing (S.B.)		
A602		22	60'-0"	(S.B.)		
A603		11	28'-0"	(Median)		
A605		11	49'-6"	(N.B.)		
A607		8	30'-6"	(N.B. & Ret. Wall)		
A608		6	43'-0"	(N.B. & Ret. Wall)		
A610		7	21'-3"	(Wing)		
A612		7	22'-9"	(Wing - Romp)		
A613		7	22'-0"	Footling (Wing - Romp)		
A620		19	5'-0"	Wing (S.B.)		
A622	*6	21	9'-9"	Ret. Wall		

A R S				100' 11 1/2" to Breastwall (touch)
A701	7	4	8'-5"	Footing-Transverse (Wings, S.B.)
A702	↑	↑	8'-6"	
A703			8'-7"	
A704			8'-8"	
A705			8'-9"	
A706			8'-10"	
A707			8'-11"	
A708			9'-0"	
A709			9'-1"	
A710			9'-2"	
A711			9'-3"	
A712			9'-4"	
A713			9'-5"	
A714			9'-6"	
A715			9'-7"	
A716			9'-8"	
A717			9'-9"	
A718		↑	9'-10"	
A719		4	9'-11"	
A720		2	10'-3"	
A721		2	10'-0" Footing-Transverse (Wing, S.B.)	
A723		696	10'-6" Footing-Transverse (S.B., N.B. & Ref. Wall)	
A724		96	7'-0" Footing-Transverse (Ramp)	
A742	7	6	6'-6" Breastwall (S.B., N.B. & Med.)	
A802	3	31	12'-6" Ref. Wall	
A902	7	174	14'-0" Breastwall (N.B. & S.B.)	

Mark	Size	Age	Length	Location
B401	"	30	5'-8"	Bearing Areas
B402	"	30	6'-8"	Bearing Areas
B503	"	14	6'-6"	Bridge Seat (S.B.)
R504	"	27	7'-11"	Bridge Seat (S.B. & N.B.)
B505	"	43	9'-3"	Bridge Seat (S.B. N.B. & Median)
B507	"	8	7'-0"	Broadwall - N. Corner (S.B.)
B510	"	3	11'-0"	Backwall - N. Corner (S.B.)
B511	"	6	11'-6"	Broadwall - N. Corner (S.B.)
B515	"	1	7'-2"	Top of Backwall
B541	"	30	5'-11"	Broadwall - N. Corner (S.B.)

E N T		B A R	
Mark	Size	No.	Length Loc. on
B241	*5	15	8" 0" Breastwall corner (N.B.)
B351	1	11	5" 1" Breastwall corner (N.B.)
B360		59	5" 9" Footing Power (Ramp & S. Wing)
B365		16	9" 4" Bridge Seat (Ramp)
B369		9	14" 0" Backwall - N. corner (Ramp)
B371	1	11	11" 9" S. corner (Ramp & N.B.)
B379	1	5	5" 2" Breastwall corner (Ramp)
B385	*5	5	5" 2" Breastwall corner (Ramp)

B605 & B 607 B801, B901

Mark	Size	No.	Length	Location
B605	"6	11	21-3"	Footring (Med.wing)
B607	"6	11	59-2"	Footring (N.B.)
B629	"6	73	4-6"	Approach S/ab Seat
B741	"7	25	9-0"	Breast wall (S.B. : N.P.)
B742	"7	30	10-0"	Breast wall (S.B. : N.E.)
B743	"7	27	10-6"	Breast wall (S.B. : Median)
B801	"8	24	12-5"	Ret. wall
B901	"9	44	12-3"	Ret. wall & North wing
B101	"11	237	9-9"	Footring Dwell

B54	5	11	5'-1"	Breastwall/Wcorner (S.B.)	S T R
B501	5	261	4'-0"	Footings & Backwall Dawels	
B502	84	13'-0"	Breastwall (N.B. & S.B.) Median		
B506	68	19'-10"	Breastwall & Backwall (S.B.)		
B507	4	17'-8"	Bridge Seat (S.B.)		
B508	70	22'-10"	Breastwall & Backwall (S.B. & N.B.)		
B513	2	12'-4"	Top of Backwall (S.B. & Median)		
B514	14	7'-0"	Backwall (S.B. & Median)		
B516	74	7'-0"	Backwall // (N.B., S.B. & Median)		
B517	8	18'-8"	Breastwall (S.B. & N.B.)		
B518	2	6'-2"	Curtain Wall (S.B.)		
B519	15	16'-6"	North Wing		
B520	23	9'-9"			
B521	12	5'-6"			
B533	22	21'-8"			
B534	2	20'-3"			
B535		16'-6"			
B536		12'-9"			
B537		9'-0"			
B538		5'-3"			
B539	2	22'-3"	North Wing		
B542	20	6'-9"	Ret. Wall		
B543	2	7'-0"	Breastwall (Median)		
B544	36	16'-9"	Breastwall & Backwall (Median)		
B545	2	16'-0"	Top of Backwall (Median)		
B597	2	6'-0"	Curtain Wall (N.B.)		
B548	36	19'-6"	Breastwall & Backwall (N.B.)		
B553	20	29'-0"	Ret. Wall		
B554	2	28'-0"			
B555	1	23'-0"			
B556		18'-3"			
B557		15'-9"			
B558	1	8'-8"			
B559	2	30'-0"			
B561	20	15'-6"			
B562	16	11'-0"	Ret. Wall		
B563	20	7'-0"	South Wing		
B564	47	9'-6"	Breastwall (Ramp)		
B566	8	25'-3"	Breastwall (Ramp)		

A I G H T			B A		
B567	*5	12	20'-0"	Breastwall & Backwall (Ramp)	
B570		59	6'-0"	Backwall & South Wing (Ramp)	
B573		2	29'-0"	Top of Backwall (Ramp)	
B574		10	8'-9"	South Wing	
B575		10	14'-8"		
B576		2	12'-8"		
B577		2	6'-6"		
B578		2	15'-6"	South Wing	
B580		18	3'-4"	Curtain Wall	
B583		2	5'-4"	Curtain Wall	
B584	*5	2	5'-2"	Curtain Wall (Ramp)	
B586	*5	2	9'-8"	South Wing	
B601	*6	20	22'-3"	Footling (S. Wing)	
B602		22	60'-0"	(S.B.)	
B603		14	48'-5"	(Ret. Wall)	
B604		11	20'-0"	(Median)	
B606		11	53'-7"	(N.B.)	
B608		8	35'-0"	(Ret. Wall)	
B609		14	30'-0"	Footling (Ramp)	
B620		29	6'-6"	North Wing	
B621		15	8'-0"	Ret. Wall	
B623		18	7'-0"	Backwall (S.B.)	
B626		28	7'-1"	Backwall (S.B. & N.B.)	
B627		30	7'-0"	Backwall (S.B., N.B. & Median)	
B628		12	7'-5"	Backwall (Median)	
B630	*6	24	5'-6"	Ret. Wall	
B701	*7	648	10'-6"	Footling (Transverse)	
B702		4	8'-5"		
B703		4	8'-6"		
B704		4	8'-7"		
B705		4	8'-8"		
B706		4	8'-9"		
B707	*7	4	8'-10"	Footling (Transverse)	

8101	"1	237	9'-9"	Footings Dowels
R S				
8708	"7	4	8'-11"	Footings (Transverse)
8709	"	"	9'-0"	
8710	"	"	9'-1"	
8711	"	"	9'-2"	
8712	"	"	9'-2"	
8713	"	"	9'-4"	
8714	"	"	9'-5"	
8715	"	"	9'-6"	
8716	"	"	9'-7"	
8717	"	"	9'-8"	
8718	"	"	9'-9"	
8719	"	"	9'-10"	
8720	"	4	9'-11"	
8721	"	2	10'-3"	
8722	"	2	10'-11"	
8724	"	82	7'-2"	Footings (Transverse)
8744	"7	6	8'-0"	Breastwall (N.B. & S.B.)
8902	"9	100	13'-0"	Breastwall (N.B. & Median)
8903	"9	69	14'-3"	Breastwall (N.B. & Median)

Reinforcing Steel to be A615 Grade 60
All dimensions are Out to Out of Bars.

STATE HIGHWAY COMMISSION
INTERSTATE 295 & RAMP CS-7
OVER
RELOCATED ST. JAMES STREET
IN THE CITY OF
PORTLAND
CUMBERLAND COUNTY

REINFORCING STEEL SCHEDULE - ABUTMENTS
SHEET 31 OF 32 AUGUSTA, MAINE July 1991

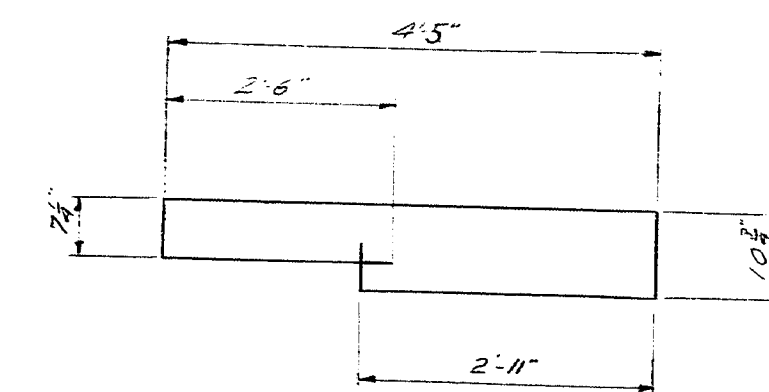
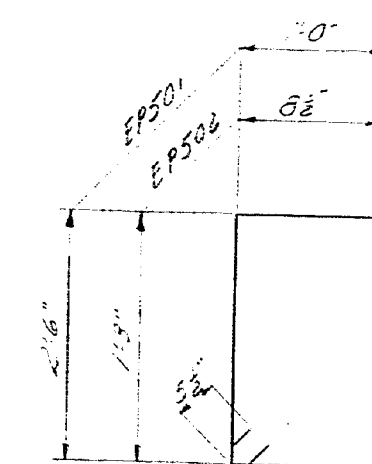
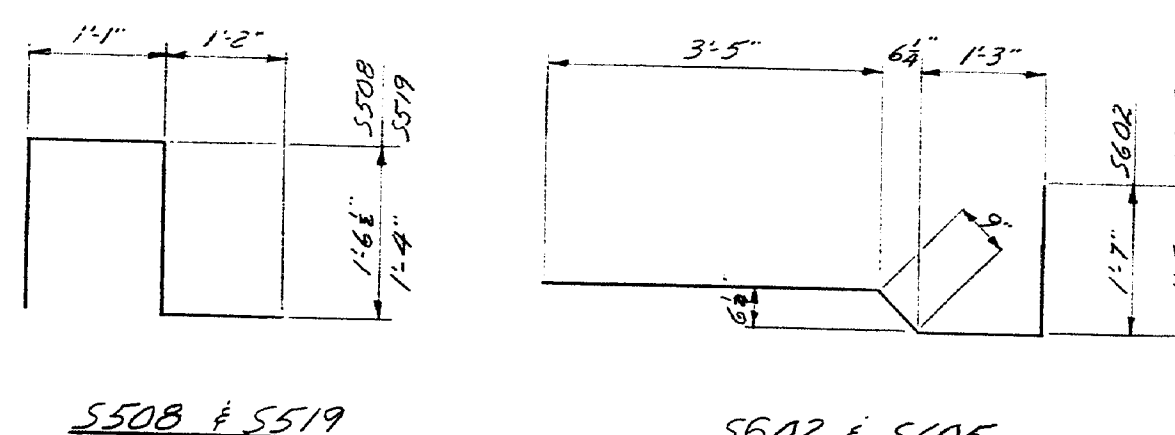
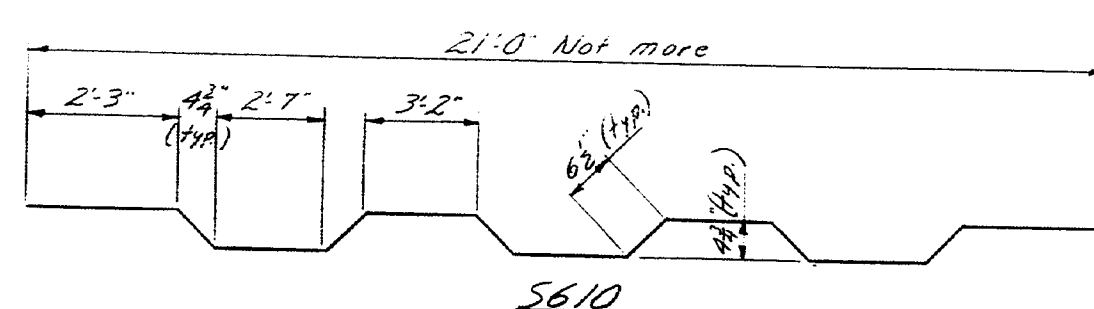
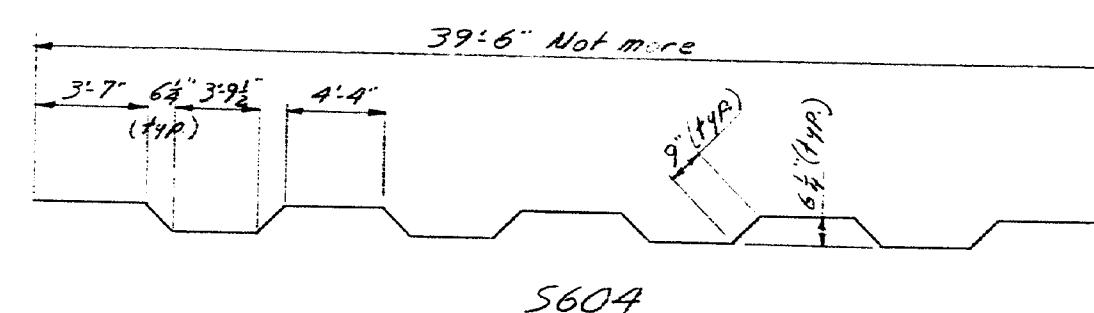
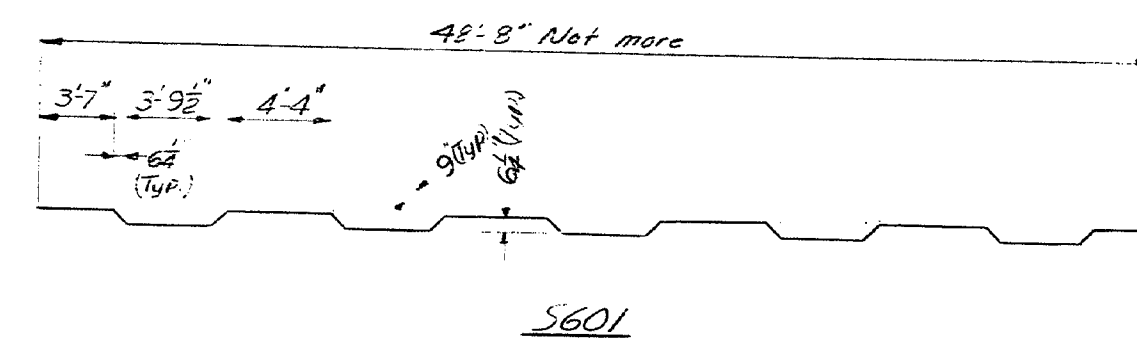
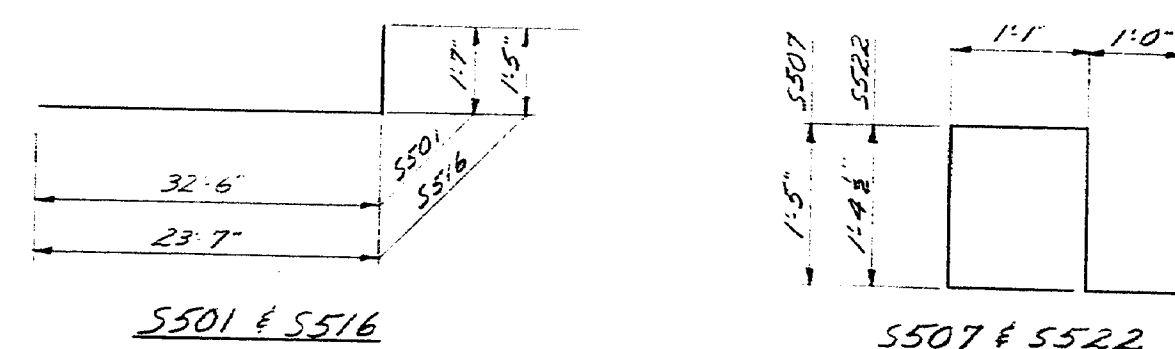
REINFORCING STEEL SCHEDULE

SUPERSTRUCTURES (NORTHBOUND, SOUTHBOUND AND RAMP)

SCHEDULE

END POSTS

S. P. R.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1295 3 (59)	37	34



EP 501 & EP 502

EP 503

BENT BARS

Mark	Size	Number	Length	Location
EP501	#5	12	7'-11"	End Posts
EP502	#5	8	4'-10"	End Posts
EP503	#5	12	12'-0"	End Posts

BENT BARS

Mark	Size	N.B.	S.B.	Ramp	Total	Length	Location
S501	#5	—	109	—	109	34'-1"	Transverse (bottom)
S507	—	109	—	—	218	6'-0"	Right Curb (N.B.) & Left Curb (S.B.)
S508	—	—	109	—	109	5'-4"	Right Curb
S516	—	109	—	—	109	25'-0"	Transverse (bottom)
S519	—	109	—	—	109	4'-11"	Left Curb
S522	#5	—	—	224	224	5'-0"	Curbs
S601	#6	—	108	—	108	50'-11"	Transverse
S602	—	—	108	—	108	7'-0"	Transverse (right side)
S604	—	108	—	—	108	41'-4"	Transverse
S605	—	108	—	—	108	6'-10"	Transverse (left side)
S610	#6	—	—	112	112	21'-10"	Transverse

STRAIGHT BARS

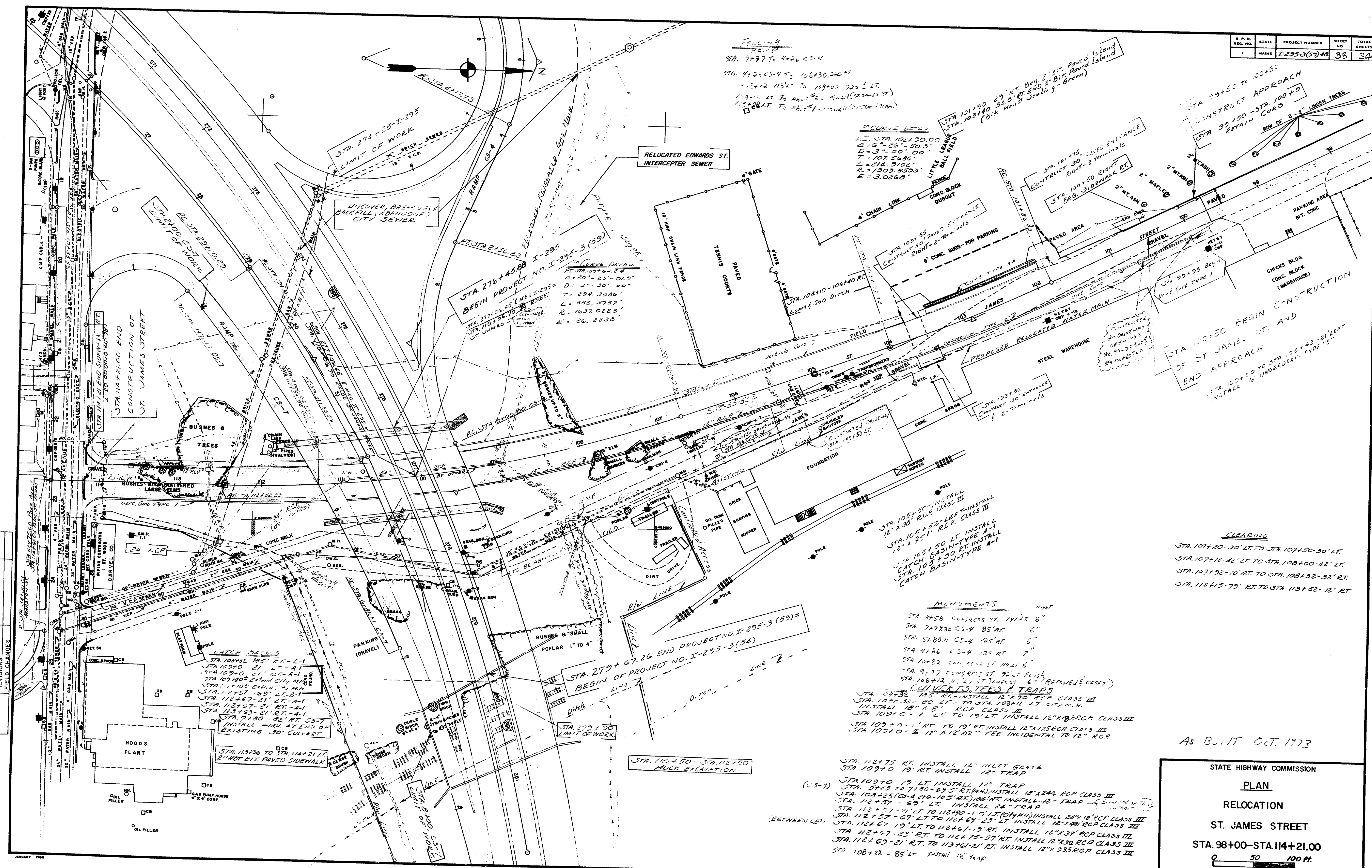
Mark	Size	N.B.	S.B.	Ramp	Total	Length	Location
S502	#5	—	109	—	109	24'-0"	Transverse (bottom)
S503	—	—	109	—	109	37'-4"	Transverse (top)
S509	—	—	109	—	109	19'-3"	Transverse (top)
S505	—	61	98	44	223	60'-0"	Longitudinal (S.B. & Ramp & N.B.)
S506	—	—	98	—	98	50'-0"	Longitudinal
S509	—	12	12	—	24	12'-11"	Curb (N.B. & S.B.)
S510	—	30	30	—	60	14'-8"	Curb (N.B. & S.B.)
S511	—	—	6	—	6	6'-5"	Curb
S512	—	—	6	—	6	6'-10"	Curb
S514	—	81	—	—	81	57'-2"	Longitudinal
S515	—	109	—	—	109	20'-0"	Transverse (top)
S517	—	109	—	—	109	27'-6"	Transverse (top)
S518	—	109	—	—	109	24'-0"	Transverse (bottom)
S520	—	—	—	44	44	54'-6"	Longitudinal
S521	—	—	—	226	226	25'-8"	Transverse (top & bottom)
S523	—	—	—	4	4	14'-2"	Curb
S524	—	—	—	24	24	14'-8"	Curb
S525	#5	—	—	4	4	6'-1"	Curb
S603	#6	—	108	—	108	7'-1"	Transverse (left side)
S606	#6	—	108	—	108	7'-8"	Transverse (right side)
S611	#6	—	—	224	224	5'-6"	Transverse (right & left side)

Reinforcing Steel to be A615 Grade 60
All dimensions are Out to Out of Bars.

STATE HIGHWAY COMMISSION
INTERSTATE 295 & RAMPCS-7
OVER
RELOCATED ST. JAMES STREET
IN THE CITY OF
PORTLAND
CUMBERLAND COUNTY
REINFORCING STEEL SCHEDULE - SUPER STR.
SHEET 32 OF 32 AUGUSTA, MAINE July 1991

153-69

SHEET NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
36	MAINE	12253(57)48	36	34



PLANS	BY	DATE
DESIGN - DETAILED	J.F.T.	11/17/70
CHECKED		
REVISIONS		
FIELD CHANGES		

STA 98+00 TO 100+50
 CONSTRUCT APPROACH
 STA 98+00 TO STA 100+50
 RETAIN CURB
 ROW OF 8" LINDEN TREES
 STA 98+00 TO STA 100+50
 CONSTRUCT APPROACH
 STA 98+00 TO STA 100+50
 RETAIN CURB
 ROW OF 8" LINDEN TREES

STA 100+50 TO 102+00
 CONSTRUCT APPROACH
 STA 100+50 TO STA 102+00
 RETAIN CURB
 ROW OF 8" LINDEN TREES
 STA 100+50 TO 102+00
 CONSTRUCT APPROACH
 STA 100+50 TO STA 102+00
 RETAIN CURB
 ROW OF 8" LINDEN TREES

STA 102+00 TO 104+00
 CONSTRUCT APPROACH
 STA 102+00 TO STA 104+00
 RETAIN CURB
 ROW OF 8" LINDEN TREES
 STA 102+00 TO 104+00
 CONSTRUCT APPROACH
 STA 102+00 TO STA 104+00
 RETAIN CURB
 ROW OF 8" LINDEN TREES

STA 104+00 TO 106+00
 CONSTRUCT APPROACH
 STA 104+00 TO STA 106+00
 RETAIN CURB
 ROW OF 8" LINDEN TREES
 STA 104+00 TO 106+00
 CONSTRUCT APPROACH
 STA 104+00 TO STA 106+00
 RETAIN CURB
 ROW OF 8" LINDEN TREES

STA 106+00 TO 108+00
 CONSTRUCT APPROACH
 STA 106+00 TO STA 108+00
 RETAIN CURB
 ROW OF 8" LINDEN TREES
 STA 106+00 TO 108+00
 CONSTRUCT APPROACH
 STA 106+00 TO STA 108+00
 RETAIN CURB
 ROW OF 8" LINDEN TREES

STA 108+00 TO 110+00
 CONSTRUCT APPROACH
 STA 108+00 TO STA 110+00
 RETAIN CURB
 ROW OF 8" LINDEN TREES
 STA 108+00 TO 110+00
 CONSTRUCT APPROACH
 STA 108+00 TO STA 110+00
 RETAIN CURB
 ROW OF 8" LINDEN TREES

STA 110+00 TO 112+00
 CONSTRUCT APPROACH
 STA 110+00 TO STA 112+00
 RETAIN CURB
 ROW OF 8" LINDEN TREES
 STA 110+00 TO 112+00
 CONSTRUCT APPROACH
 STA 110+00 TO STA 112+00
 RETAIN CURB
 ROW OF 8" LINDEN TREES

STA 112+00 TO 114+21.00
 CONSTRUCT APPROACH
 STA 112+00 TO STA 114+21.00
 RETAIN CURB
 ROW OF 8" LINDEN TREES
 STA 112+00 TO 114+21.00
 CONSTRUCT APPROACH
 STA 112+00 TO STA 114+21.00
 RETAIN CURB
 ROW OF 8" LINDEN TREES

CLEARING
 STA 107+20-30' LT. TO STA 107+50-30' LT.
 STA 107+72-42' LT. TO STA 108+00-42' LT.
 STA 107+32-10' RT. TO STA 108+32-32' RT.
 STA 112+15-79' RT. TO STA 113+82-12' RT.

MONUMENTS
 STA 1456 CONGRESS ST. 141' LT. 8"
 STA 749230 CS-4 85' RT. 6"
 STA 58011 CS-4 125' RT. 6"
 STA 4426 CS-4 125' RT. 7"
 STA 10432 CONGRESS ST. 141' LT. 8"
 STA 9332 CONGRESS ST. 92' LT. 8"
 STA 108412 CONGRESS ST. 141' LT. 8" (REMOVED) 8"
 STA 108412 CONGRESS ST. 141' LT. 8" (REMOVED) 8"

CULVERTS, TIES & TRAPS
 STA 108+32-125' RT. INSTALL 12" X 30' RCP CLASS III
 STA 108+32-125' RT. INSTALL 12" X 30' RCP CLASS III
 STA 108+32-125' RT. INSTALL 12" X 30' RCP CLASS III
 STA 108+32-125' RT. INSTALL 12" X 30' RCP CLASS III
 STA 108+32-125' RT. INSTALL 12" X 30' RCP CLASS III
 STA 108+32-125' RT. INSTALL 12" X 30' RCP CLASS III

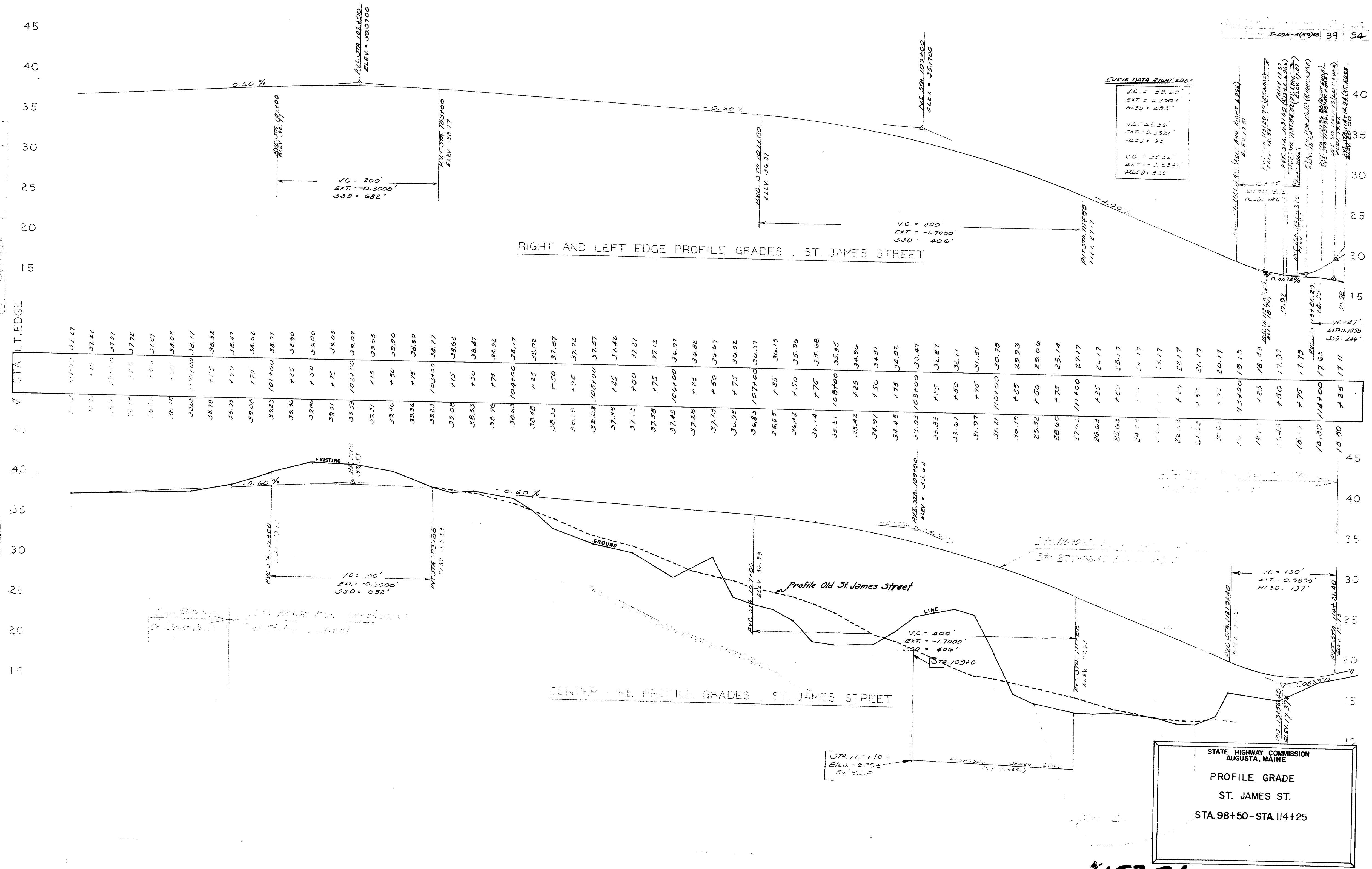
STA 112+75 RT. INSTALL 12" INLET GRATE
 STA 109+00 19' RT. INSTALL 12" TRAP
 STA 109+00 19' RT. INSTALL 12" TRAP
 STA 109+00 19' RT. INSTALL 12" TRAP
 STA 109+00 19' RT. INSTALL 12" TRAP
 STA 109+00 19' RT. INSTALL 12" TRAP
 STA 109+00 19' RT. INSTALL 12" TRAP

As Built Oct. 1973

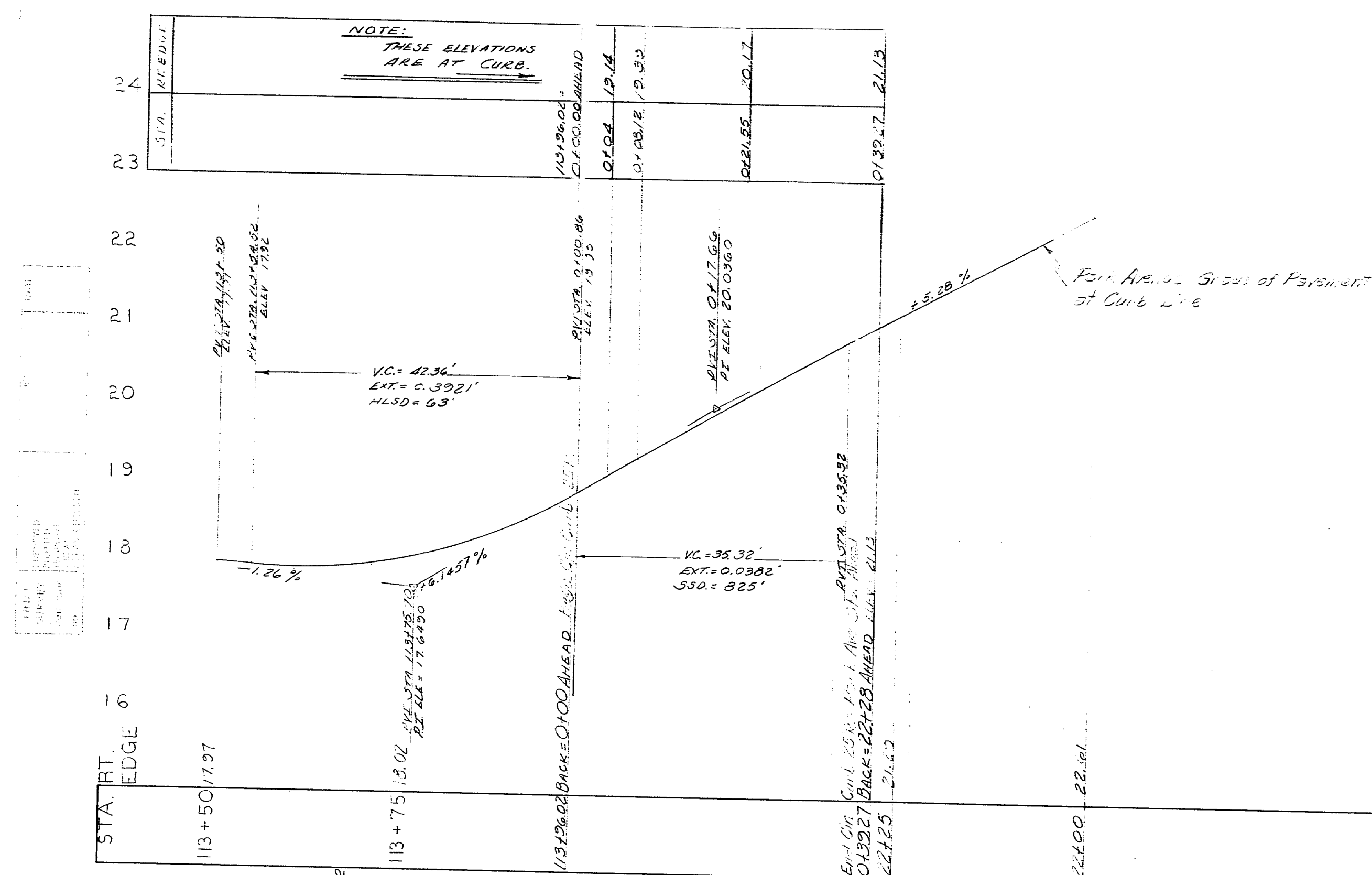
STATE HIGHWAY COMMISSION
 PLAN
 RELOCATION
 ST. JAMES STREET
 STA. 98+00-STA. 114+21.00
 Scale
 AUGUSTA, MAINE JULY 1971
 SHEET OF
 153-70
 PORTLAND - I-295

PROJECT: ST. JAMES STREET
 LOCATION: AUGUSTA, MAINE
 DRAWN BY: [Name]
 CHECKED BY: [Name]
 DATE: [Date]

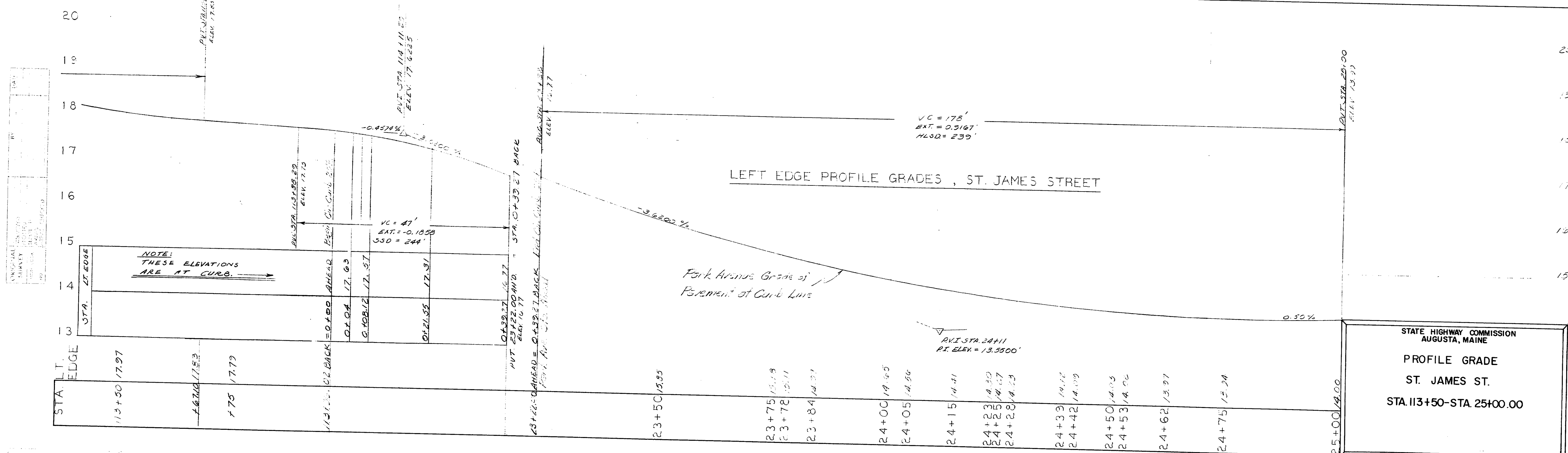
SHEET NO. 153-71
 OF 153-71
 SCALE: 1" = 40'



153-71 Portland I-295



RIGHT EDGE PROFILE GRADES , ST. JAMES STREET



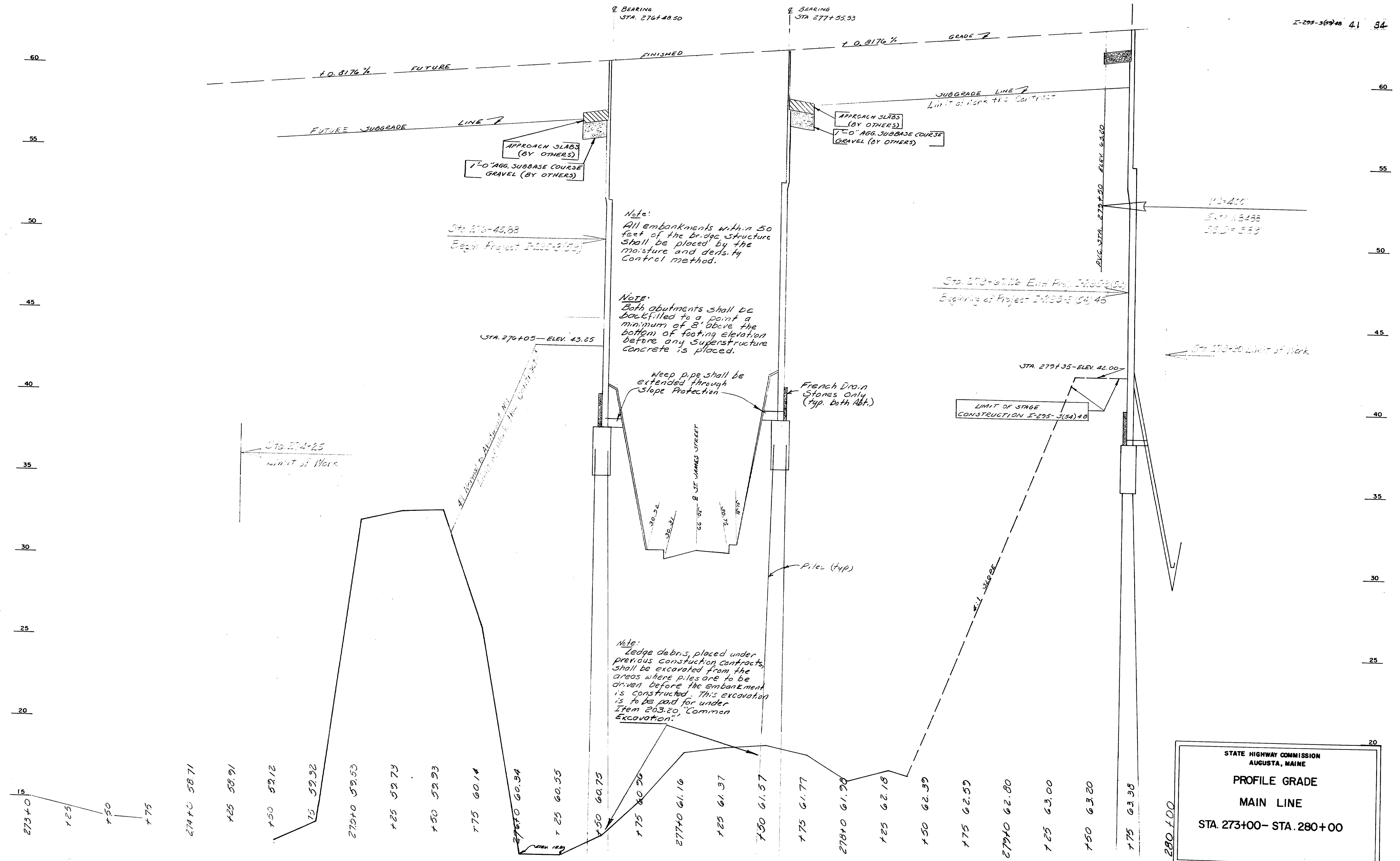
STATE HIGHWAY COMMISSION
AUGUSTA, MAINE

PROFILE GRADE

ST. JAMES ST.

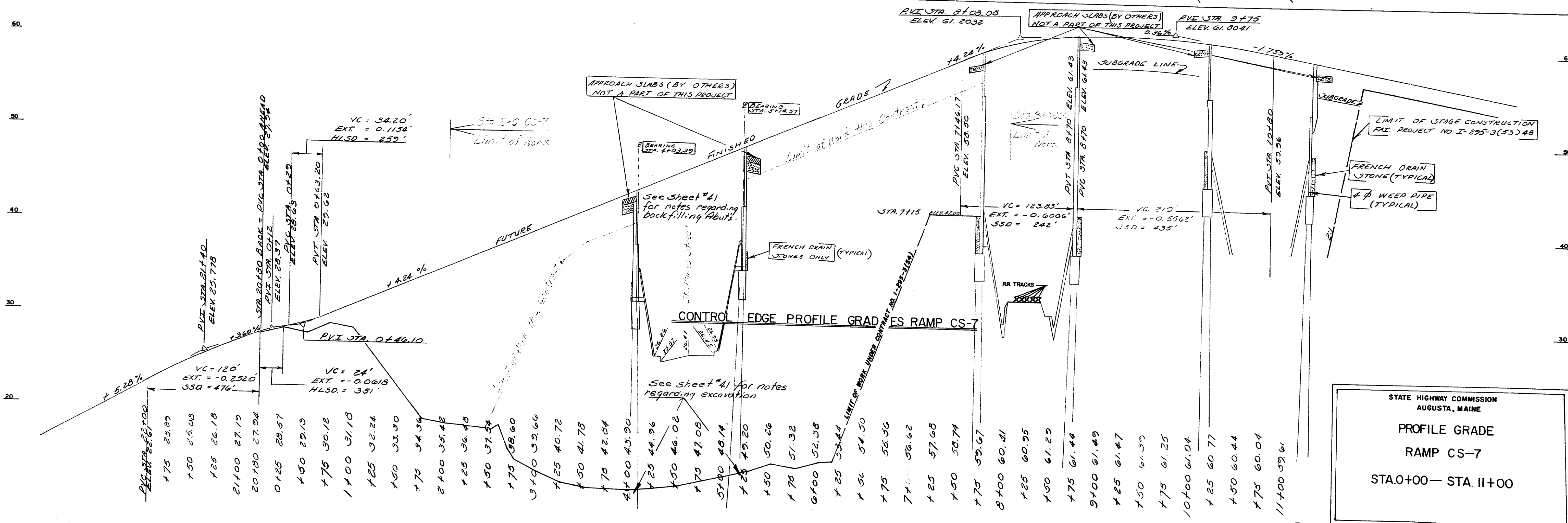
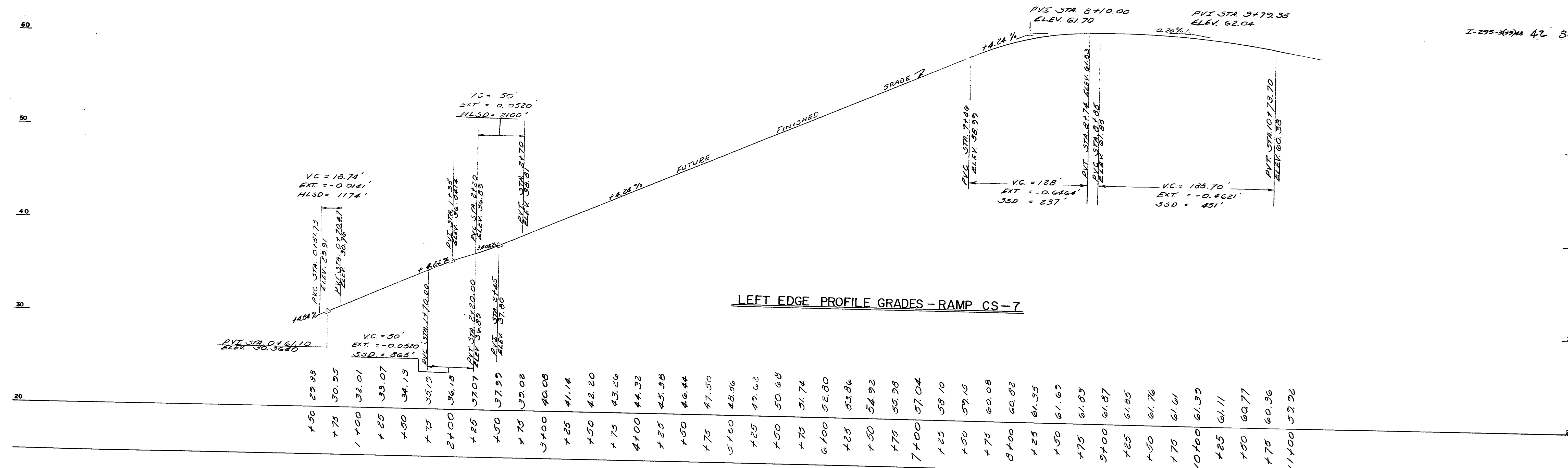
STA. 113+50-STA. 25+00.00

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153-73 Portland I-295

STATE HIGHWAY COMMISSION
AUGUSTA, MAINE
PROFILE GRADE
MAIN LINE
STA. 273+00 - STA. 280+00



STATE HIGHWAY COMMISSION
AUGUSTA, MAINE
PROFILE GRADE
RAMP CS-7
STA. 0+00 - STA. 11+00

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